

Department of Administration / Division of Purchases  
One Capitol Hill, Providence, Rhode Island 02908-5855  
Web Site: [www.purchasing.ri.gov](http://www.purchasing.ri.gov)

Date: 4/14/11

Addendum # 7448315A6

BID # 7448315A6

Title: **DESIGN/BUILD** Services for the Replacement of the Laurel Avenue Bridge  
#397, Coventry, RI

DUE DATE: **FRIDAY, APRIL 22, 2011 no later than 11:30 A.M. (Eastern).**

The following Addendum is issued with regard to the following:

A. REVISIONS/CLARIFICATIONS - See Attachment

A handwritten signature in black ink, appearing to read "Jerome D. Moynihan".

Jerome D. Moynihan, C.P.M., CPPO  
Assistant Director for Special Projects

April 14, 2011  
STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION

BID NO. 7448315

REQUEST FOR PROPOSALS  
DESIGN-BUILD SERVICES FOR THE REPLACEMENT OF THE  
LAUREL AVENUE BRIDGE #397  
COVENTRY, RI

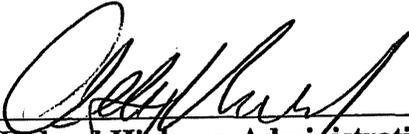
NOTICE TO PROSPECTIVE BIDDERS

**ADDENDUM NO. 6**– Prospective Respondents and all concerned are hereby notified of the following clarifications with regard to the Request for Proposals for Design-Build Services for the Replacement of the Laurel Avenue Bridge #397, Coventry, RI. This Addendum shall be incorporated in the Respondents Proposal, and shall become an integral part of the final Contract Document.

If any discrepancy occurs between verbal responses given during the March 11, 2011 Pre-Proposal Meeting and this ADDENDUM NO. 6, this ADDENDUM NO. 6 shall govern.

**A. REVISIONS/CLARIFICATIONS**

1. RFP Part 1: Replace P. 20 of 32 R-2 with the attached revised P. 20 of 32 R-3. The first paragraph in Section 4.3 has been revised.
2. RFP Part 1: Replace P. 21 of 32 with the attached revised P. 21 of 32 R-1. The DBE Form Submission Instructions paragraph has been revised.
3. RFP CD, DBE Forms: Replace the DBE Utilization Plan form with the attached revised DBE Utilization Plan Design/Build form R-1. The form has been revised.
4. RFP CD, DBE Forms: Insert new attached form entitled Affidavit of Prime Contractor.

  
Federal Highway Administration  
Division Administrator

  
Kazem Farhoumand, P.E.  
Chief Engineer

- 2) A Proposal Schedule, conforming to the WBS set forth in above, which shall include: (a) a narrative description of the proposed Schedule; (b) logic relationships, durations, critical path based on the longest path, interim milestones, and timing of the WBS elements for design and construction; and (c) environmental permitting, (d) ROW not already provided by RIDOT, and (e) design review by RIDOT. The Preliminary Proposal Schedule to be included with the Technical Proposal shall be developed using Primavera Project Planner (P3) Software. The requirements noted in this section apply exclusively to the Preliminary Proposal Schedule for the Qualifications/Technical Proposal. The successful Respondent's formal work Schedule (Level A) shall be in accordance with the Standard Specifications. Schedules containing a Bridge Open Date earlier than December 1, 2012 will weigh significantly in the evaluation of this criterion PROJECT CONTROLS.
  
- 3) Respondent's understanding of the biggest risks and challenges to attaining the contractual completion dates and what can be done to mitigate such risks and challenges.

### 4.3 DBE and OJT Requirements

#### Disadvantaged Business Enterprises ("DBE") and Training.

DBE GOAL: Respondents must provide a **written statement (one for each consultant, subconsultant, and subcontractor)**, RIDOT's **Letter of Intent to Perform as a Subcontractor** form, signed by authorized representatives of the Respondent Team, that Respondent is committed to meeting or exceeding a total of **FOURTEEN PERCENT (14%)** Disadvantaged Business Enterprise (DBE) participation goal for construction qualifying work, and **TEN (10%)** percent for consultant qualifying work with Rhode Island certified firms during design and construction of the Project. RFP documentation will require full disclosure of RI certified DBE Firm(s), assigned task(s), current certification and DBE contract cost, using the **RIDOT DBE Utilization Plan Design/Build** form *to be submitted with Price Proposal*. DBE Certification letters issued by the State may be submitted post-qualification. DBE participation shall consist of concerted efforts by the Respondent as part of its affirmative action responsibilities to include DBE Firms on Federal-Aid transportation projects. Hereafter, DBE refers to businesses owned or controlled by socially and economically disadvantaged person(s) as certified by the RIDOA for the RIDOT.

A list of current Rhode Island State certified DBE firms may be obtained through the State's Minority Business Enterprise (MBE) Office website @ [www.mbe.ri.gov](http://www.mbe.ri.gov). Any questions should be directed to:

**RIDOT Office of Business and Community Resources  
Room 107, Two Capitol Hill  
Providence, RI 02903  
(401) 222-3260**

- **TRAINEES:** The Respondent must also provide a **written statement, RIDOT's OJT Acknowledgement/Statement of Intent** form signed by an authorized representative of the Respondent that the Respondent will develop and maintain a continuous on-the-job-training (OJT) program achieving the required Training hours. If the Respondent is selected for the Project, they must provide a RIDOT approved OJT Plan before award.

The trainee hours for this project are based on 18 MONTHS for completion and 1,500 hours for OJT reimbursement.

DBE Form Submission Instructions:

The **Letter of Intent to Perform as a Subcontractor** and the **OJT Acknowledgement/Statement of Intent** forms (WITHOUT dollar amounts) are to be submitted as part of the Qualifications/Technical Proposal. The **DBE Utilization Plan Design/Build and the Affidavit of Prime Contractor** forms are to be submitted with the Price Proposal only.

#### 4.4 Required Forms

Besides the RIVIP Bidder Certification Cover Sheet as required at the State level and obtained through the RIVIP website, RIDOT also requires that the following FIVE (5) FORMS be completed and included in your submission package in line with federal regulations and departmental policy. These FORMS will be reviewed for completeness and will be made part of the contract documents.

- **DEBARMENT FORM:** Must be completed and signed by an authorized agent of the Bidder.
- **LOBBYING FORM:** Enter known project information on PAGE 1 (DESCRIPTION etc.); Respondent must complete FORM and submit signed by an authorized individual of the Bidder.
- **CONFLICTS DISCLOSURE STATEMENT:** In line with directions stated, completed FORM(s) must be signed and submitted accordingly.
- **ANTI-COLLUSION CERTIFICATE:** Must be completed and signed by an authorized agent of the Bidder.
- **W-9 Form:** Must be completed and signed by an authorized agent of the Respondent. Form may be downloaded at [www.purchasing.ri.gov](http://www.purchasing.ri.gov).

**ALL FORMS (Except W-9) ARE ON THE RFP CD AND MUST BE COMPLETED AND COPIES SUBMITTED ALONG WITH EACH QUALIFICATIONS/TECHNICAL PROPOSAL SUBMISSION. ("ORIGINAL" & COPIES). PLEASE NOTE, FOR W-9 FORM ONLY, ONE (1) UNBOUND "ORIGINAL" COPY IS REQUIRED AT TIME OF SUBMISSION. COPIES OF W-9 NEED NOT BE INCLUDED IN INDIVIDUAL PROPOSAL SUBMISSIONS.**

#### 4.5 Price Proposal

## DBE UTILIZATION PLAN Design/Build

Project Name: D/B Services Replacement of Laurel Avenue Bridge  
#397, Coventry, RI

I, JOHN McNamee HEREBY DECLARE AND AFFIRM that I am the  
Vice President (Title) - duly authorized representative) with  
A.A. Wilco Corp (Name of Prime Contractor), and that I have personally

reviewed the material and facts set forth in and submitted with this DBE Utilization Plan.

The following correctly represents our anticipated commitment to the DBE participation on this contract.

Name DBE Firms and Address	Type of Work to be Performed Contract Amount	* DBE Contract Amount
<u>Lamson Engineering Corp.</u> <u>437 Cherry St. Newton, MA</u>	<u>Geotechnical Design</u>	<u>\$ 39,411.79</u>
<u>Narragansett Engineering</u> <u>310 1/2 E Main Rd. Portsmouth, RI</u>	<u>Surveying</u>	<u>\$ 15,793.00</u>
<u>M.O.W. Landscaping, Inc.</u> <u>PO Box 70220</u> <u>N. Dartmouth, MA</u>	<u>Landscaping</u>	<u>\$ 8,000.00</u>
<u>F.C. Construction Corp.</u> <u>133 State Road</u> <u>Westport, MA</u>	<u>Curbing, Sidewalks, Stone</u> <u>Work</u>	<u>\$ 235,000.00</u>
<u>Atlantic Bridge &amp; Engineering</u> <u>191 Elm St.</u> <u>Salisbury, MA</u>	<u>Box Beam Erection</u>	<u>\$ 320,000.00</u>
<u>D.M. Concrete Floor</u> <u>2 Lark St.</u> <u>Fall River, MA</u>	<u>furnish &amp; Place Concrete</u>	<u>\$ 100,000.00</u>
<u>Schedule Dynamics, Inc.</u> <u>15 Wake Robin Rd.</u> <u>Sudbury, MA</u>	<u>Scheduling</u>	<u>\$ 20,000.00</u>

\* List amount if known

Signature: [Signature]

Date: 4/22/11

### AFFIDAVIT OF PRIME CONTRACTOR

To the best of my knowledge, information and belief, the facts and representations contained in the aforementioned DBE Utilization Plan are true and no material facts have been omitted. The undersigned will enter into formal agreements with the DBE firms listed in the attached DBE Utilization Plan. for this design /build project.

The A.A. Will Corporation (Prime Contractor) designates the following person as the Manager, Office of Contract Compliance:

JOHN McNAMARA  
(Name – Please Print or Type)

781-341-4800  
(Phone Number)

I do solemnly declare and affirm under the penalties of perjury that the contents of this document and attachments are true and correct. I am authorized on behalf of the above firm, to make this affidavit.

A.A. Will Corporation  
(Name of Prime Contractor – Print or Type)

Affidavit of Prime Contractor  
(Name of Affidavit)

4/21/11  
Date Authorized Signature

[Signature]

Title Vice President

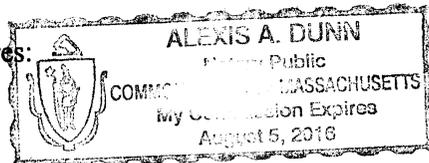
State of Massachusetts  
County of Norfolk

On this 21 day of April, 2011 before me, the undersigned officer, personally appeared John McNamara, known to me to be the person described in the foregoing Affidavit, and acknowledged that he (she) executed the same in the capacity therein stated and for the purposed therein contained.

In witness thereof, I hereunto set my hand and official seal.

Alexis A. Dunn (Notary Public)

My commission expires:



Price Proposal Form			
Item No.	Description	Unit	Total
D-1	Preliminary Engineering	lump sum	\$ 140,405.00
D-2	Historical/Environmental Coordination	lump sum	\$ 13,000.00
D-3	Foundation Design	lump sum	\$ 43,544.00
D-4	Substructure Design	lump sum	\$ 45,700.00
D-5	Superstructure Design	lump sum	\$ 56,690.00
D-6	Profile/Highway Design	lump sum	\$ 36,340.00
D-7	Utilities	lump sum	\$ 25,770.00
C-1	Mobilization	lump sum	\$ 400,000.00
C-2	Structure Monitoring	lump sum	\$ 25,000.00
C-3	Site Preparation	lump sum	\$ 50,000.00
C-4	Demolition	lump sum	\$ 60,000.00
C-5	Utilities	lump sum	\$ 160,000.00
C-6	Foundations	lump sum	\$ 747,000.00
C-7	Substructures	lump sum	\$ 520,000.00
C-8	Superstructure	lump sum	\$ 931,000.00
C-9	Roadway Approaches	lump sum	\$ 300,000.00
C-10	Reconstruct River Bed Splash Pad	lump sum	\$ 1,083,000.00
C-11	New Spillway Walls/Stone Fascias	lump sum	\$ 526,000.00
<b>Total Cost of Project: (Bidder shall specify price information in both words and numbers)</b>			
<b>Lump Sum: Five Million Two Hundred Twenty One Thousand Four Hundred Seventy Five and 20/100ths</b>			<b>\$ 5,221,475.00</b>

D: Denotes Design Items; C: Denotes Construction Items

Owner/President Signature: *[Signature]* A.A. Wick Cop Date: 4/22/11

Address: 145 Grand Street  
Stearns MA 02072

Design/Build Replacement of Laurel Avenue Bridge No 397  
Coventry, Rhode Island  
Rhode Island Contract No 2010-DF-055  
A. A. Will Corporation

Monthly Payment Schedule

1-Jul-2011	\$250,000.00
1-Aug-2011	\$200,000.00
1-Sep-2011	\$160,000.00
1-Oct-2011	\$70,000.00
1-Nov-2011	\$70,000.00
1-Dec-2011	\$80,000.00
1-Jan-2012	\$120,000.00
1-Feb-2012	\$120,000.00
1-Mar-2012	\$120,000.00
1-Apr-2012	\$120,000.00
1-May-2012	\$120,000.00
1-Jun-2012	\$120,000.00
1-Jul-2012	\$590,000.00
1-Aug-2012	\$590,000.00
1-Sep-2012	\$590,000.00
1-Oct-2012	\$590,000.00
1-Nov-2012	\$590,000.00
1-Dec-2012	\$590,000.00
1-Jan-2013	\$20,000.00
1-Feb-2013	\$20,000.00
1-Mar-2013	\$20,000.00
1-Apr-2013	\$20,000.00
1-May-2013	\$20,000.00
1-Jun-2013	\$20,000.00
1-Jul-2013	\$11,475.00
Total	\$5,221,475.00

State of Rhode Island and Providence Plantations Contract Offer  
**RIVIP GENERATED BIDDER CERTIFICATION COVER FORM**

SECTION 1 - VENDOR INFORMATION

Bid/RFP Number: 7448315A6  
Bid/RFP Title: RFP - DESIGN / BUILD SVCS FOR REPLACEMENT OF LAUREL AVE BRIDGE #397 (ADDEN  
Opening Date & Time: 4/22/2011 11:30 AM  
RIVIP Vendor ID #: 43903  
Vendor Name: A. A. Will Corporation  
Address: 145 Island Street  
Stoughton, MA 02072  
USA  
Telephone: (781) 341-4800  
Fax: (781) 297-9776  
E-Mail: Estimating@AAWill.com  
Contact Person: John McNamara  
Title: Vice-President  
R.I. Foreign Corp #:

\*\*\*NOTICE TO VENDORS\*\*\*

Effective January 1, 2011 all public works project related bids or proposals exceeding one million (\$1,000,000) dollars are required to include a "public copy". All agency contract solicitations, requests for proposals, invitations for bids, etc. shall state that any bid or proposal that exceeds one million (\$1,000,000) dollars must include a copy to be available for public inspection upon the opening of the bids. Any bid or proposal in excess of one million (\$1,000,000) dollars which does not include a copy for public inspection shall be deemed to be non-responsive. For further information, please see R. I. Gen. Laws §37-2-18 (P.L. 221) <http://www.rilin.state.ri.us/PublicLaws/law10/law10221.htm> and Purchasing Rules & amendment at <http://www.purchasing.ri.gov/Notices2.aspx>. See Question #11 below for further instructions regarding RIDOT Highway and Bridge Construction projects.

In addition, the Division of Purchases has promulgated proposed regulations pursuant to R.I. Gen. Laws § 37-2-18 that implements the "public copy" requirement. Public hearing on the proposed regulations was held on Friday, December 17, 2010. The proposed regulations became final on January 11, 2011. For further information please visit [www.sos.ri.gov](http://www.sos.ri.gov).

NOTE: AWARD OF CONTRACTS AND PURCHASE ORDERS SHALL BE SUBJECT, AT THE DISCRETION OF THE PURCHASING AGENT, TO THE OFFEROR COMPLETING AN ON-LINE RIVIP REGISTRATION at [www.purchasing.ri.gov](http://www.purchasing.ri.gov). It is THE RESPONSIBILITY OF THE VENDOR to make on-line corrections/updates using the Vendor maintenance program on the RI Division of Purchases Web Site.

SECTION 2 - REQUIREMENTS

ALL OFFERS ARE SUBJECT TO THE REQUIREMENTS, PROVISIONS AND PROCEDURES CONTAINED IN THIS THREE-PAGE CERTIFICATION FORM. Offerors are expected to READ, SIGN and COMPLY with all requirements. Failure to do so may be grounds for disqualification of the offer contained herein.

Section 2.1 - RULES FOR SUBMITTING OFFERS

2.1A. This CERTIFICATION FORM MUST BE ATTACHED IN ITS ENTIRETY TO THE FRONT OF THE OFFER and shall be considered an integral part of each offer made by a vendor to enter into a contract with the State of Rhode Island, Division of Purchases. As such, submittal of the entire Bidder Certification Cover Form, signed by a duly authorized representative of the offeror attesting that he/she (1) read and agrees to comply with the requirements set forth herein and (2) to the accuracy of the information provided and the offer extended, is a mandatory part of any contract award.

To assure that offers are considered on time, each offer must be submitted with the specific Bid/RFP/LOI number (provided above), date and time of opening marked in the upper left hand corner of envelope. Each bid/offer must be submitted in separate sealed envelopes.

A complete, signed (in ink) offer package, must be delivered to the Division of Purchases (via any mail or messenger service) by the time and date specified for the opening of responses in a sealed envelope.

Bids must be submitted on the RI bid solicitation forms provided, indicating brand and part numbers of items offered, as appropriate. Bidders must submit detailed cuts and specs on items offered as equivalent to brands requested WITH THE OFFER. Bidders must be able to submit samples if requested.

ii To: Division of Purchases, One Capitol Hill, Second Floor, Providence, RI 02908-5855.

Documents misdirected to other State locations or which are not present in the Division of Purchases at the time of opening for whatever cause will be deemed to be late and will not be considered. For the purposes of this requirement, the official time and date shall be that of the time clock in the Division of Purchases. Postmarks shall not be considered proof of timely submission.

2.1B. RIVIP SOLICITATIONS. To assure maximum access opportunities for users, public bid/RFP solicitations shall be posted on the RIVIP for a minimum of seven days and no amendments shall be made within the last five days before the date an offer is due. Except when access to the Web Site has been severely curtailed and it is determined by the State Purchasing Agent that special circumstances preclude extending a solicitation due date, requests to mail or fax hard copies of solicitations will not be honored. When the result of an Internet solicitation is unsuccessful, the State of Rhode Island will cancel the original solicitation and resolicit the original offer directly from vendors.

2.2. PRICING. Offers are irrevocable for sixty (60) days from the opening date (or such other extended period set forth in the solicitation) and may not be withdrawn, except with the express permission of the State Purchasing Agent. All pricing will be considered to be firm and fixed unless otherwise indicated. The State of Rhode Island is exempt from Federal excise taxes and State Sales and Use Taxes. Such taxes shall not be included in the bid price. PRICES QUOTED ARE FOB DESTINATION.

2.3. DELIVERY and PRODUCT QUALITY. All offers must define delivery dates for all items; if no delivery date is specified, it is assumed that immediate delivery from stock will be made. The contractor will be responsible for delivery of materials in first class condition. Rejected materials will be at vendor's expense.

2.4. PREVAILING WAGE, OSHA and APPRENTICESHIP.

2.4.1 Prevailing Wage and OSHA Safety Training Requirements. The provisions of the State labor laws and OSHA Safety Training, including but not limited to Rhode Island General Laws 37-13-1 et seq. and 28-20-1 et seq., shall apply for all public works contracts. Prevailing wage rates are posted in the information section of the RIVIP. The RI Department of Labor and Training should be contacted for regulatory requirements.

2.4.2 (a) Apprenticeship. Rhode Island General Laws §37-13-3.1 requires all general contractors and subcontractors who perform work on any public works contract awarded by the state valued at one million dollars (\$1,000,000) or more shall employ apprentices required for the performance of the awarded contract. The number of apprentices shall comply with the apprentice to journeyman ratio for each trade approved by the apprenticeship council of the department of labor and training.

2.4.2(b) In addition to executing this certification, the general contractor shall be responsible for requiring that all subcontractors on the awarded project certify their compliance with R.I. Gen. Laws §37-13-3.1 prior to allowing the subcontractor to commence work on the awarded project. The general contractor shall be responsible for submitting the subcontractors compliance certification to the Division of Purchases after the contracts are finalized between the contractor and subcontractor.

2.5. PUBLIC RECORDS. Offerors are advised that all materials submitted to the State for consideration in response to this solicitation will be considered without exception to be Public Records pursuant to Title 38 Chapter 2 of the Rhode Island General Laws, and will be released for inspection immediately upon request once an award has been made. Offerors are encouraged to attend public bid/RFP openings to obtain information; however, bid/RFP response summaries may be reviewed after award(s) have been made by using the RIVIP at any time or appearing in person at the Division of Purchases Mondays through Fridays between 8:30 a.m. and 3:30 p.m. Telephone requests for results will not be honored. Written requests for results will only be honored if the information is not available on the RIVIP.

### SECTION 3 - AWARD DETERMINATION

Award will be made to the responsive and responsible offeror quoting the lowest net price in accordance with specifications, for any individual item(s), for major groupings of items, or for all items listed, at the State's sole option.

3.1. BID SURETY. Where bid surety is required, bidder must furnish a bid bond or certified check for 5% of the bid total with the bid, or for such other amount as may be specified. Bids submitted without a required bid surety will not be considered.

3.2. SPECIFICATIONS. Unless specified "no substitute," product offerings equivalent in quality and performance will be considered (at the sole option of the State) on the condition that the offer is accompanied by detailed product specifications. Offers which fail to include alternate specifications may be deemed nonresponsive.

### SECTION 4 - CONTRACT PROVISIONS

4.1. VENDOR AUTHORIZATION TO PROCEED.

4.1A. When a purchase order, change order, contract/agreement or contract/agreement amendment is issued by the RI Division of Purchases, no claim for payment for services rendered or goods delivered contrary to or in excess of the contract terms and scope shall be considered valid unless the vendor has obtained a written change order or contract amendment issued by the Division of Purchases PRIOR TO delivery.

4.1B. Any offer, whether in response to a solicitation for proposals or bids, or made without a solicitation, which is accepted in the form of an order OR Pricing Agreement made in writing by the Purchasing Agent, or a state official with purchasing authority delegated by the Purchasing Agent, shall be considered a binding contract.

4.2. REGULATIONS, GENERAL TERMS AND CONDITIONS GOVERNING STATE CONTRACTS. This solicitation and any contract or purchase order arising from it are issued in accordance with the specific requirements described herein, and the State's Purchasing Laws and Regulations and other applicable State Laws. The Regulations, General Terms and Conditions are incorporated into all state contracts. These regulations and basic information on How To Do Business with the State of Rhode Island are posted on the Rhode Island Vendor Information Program Website ([www.purchasing.ri.gov](http://www.purchasing.ri.gov)).

4.2A. ARRA SUPPLEMENTAL TERMS AND CONDITIONS. Contracts and sub-awards funded in whole or in part by the American Recovery and Reinvestment Act of 2009, Pub.L.No. 111-5 and any amendments thereto, such contracts and sub-awards, shall be subject to the Supplemental Terms and Conditions For Contracts and Sub-awards Funded in Whole or in Part by the American Recovery and Reinvestment Act of 2009, Pub.L.No. 111-5 and any amendments thereto located on the Division of Purchases website at [www.purchasing.ri.gov](http://www.purchasing.ri.gov).

4.3. EQUAL EMPLOYMENT OPPORTUNITY. Compliance certificate and agreement procedures will apply to all awards for supplies or services valued at \$10,000 and more. Minority Business Enterprise policies and procedures, including subcontracting opportunities as described in Title 37 Chapter 14.1, of the Rhode Island General Laws, also apply.

Revised: 3/21/11

4.4. PERFORMANCE BONDS. Where indicated, successful bidder must furnish a 100% performance bond and labor and payment bond for contracts subject to Title 37 Chapters 12 and 13 of the Rhode Island General Laws. All bonds must be furnished by a surety company authorized to conduct business in the State of Rhode Island. Performance bonds must be submitted within 21 calendar days of the issuance of a tentative notice of award.

4.5. DEFAULT and NON-COMPLIANCE. Default and/or non-compliance with the RIVIP requirements and any other aspects of the award may result in withholding of payment(s), contract termination, debarment, suspension, or any other remedy necessary that is in the best interest of the state.

4.6. COMPLIANCE. Vendor must comply with all applicable federal, state and local laws, regulations and ordinances.

4.7. SPRINKLER IMPAIRMENT AND HOT WORK. The Contractor agrees to comply with the practices of the State's insurance carrier for sprinkler impairment and hot work. Prior to performing any work, the Contractor shall obtain the necessary information for compliance from the Risk Management Office at the Department of Administration or the agency for which work will be performed.

**SECTION 5 - CERTIFICATIONS AND DISCLOSURES**  
**ALL CONTRACT AWARDS ARE SUBJECT TO THE FOLLOWING DISCLOSURES & CERTIFICATIONS**

**Offerors must respond to every disclosure statement.**

**A person authorized to enter into contracts must sign the offer and attest to the accuracy of all statements.**

Indicate Yes (Y) or No (N):

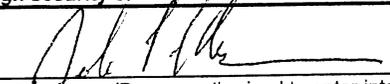
1. Has your firm (or any principal) been subject to any of the following findings by the Federal Government, the State of Rhode Island or any other jurisdiction? Suspension, Debarment, Indictment, Criminal Conviction. CIRCLE APPROPRIATE ITEM(S).
2. Has your firm (or any principal) been fined more than \$5000 for a single violation by the Rhode Island Department of Environmental Management for violation of Rhode Island Wetlands law?
3. I/we certify that I/we will immediately disclose, in writing, to the Chief Purchasing Officer any potential conflict of interest, which may occur during the course of the engagement authorized pursuant to this contract.
4. I/we acknowledge that, in accordance with Chapter 37-2-54(c) of the Rhode Island General Laws "no purchase or contract shall be binding on the state or any agency thereof unless approved by the Department [of Administration] or made under general regulations which the Chief Purchasing Officer may prescribe", including change orders and other types of contracts and under State Purchasing Regulation 8.2.1.1.2, "any alleged oral agreement or arrangements made by a bidder or contractor with any agency or an employee of the Office of Purchases may be disregarded and shall not be binding on the state".
5. I/we certify that the above vendor information is correct and complete.
6. I/we certify that I/we or my/our firm possesses all licenses required by Federal and State laws and regulations as they pertain to the requirements of the solicitation and offer made herein and shall maintain such required license(s) during the entire course of the contract resulting from the offer contained herein and should my/our license lapse or be suspended, I/we shall immediately inform the Rhode Island State Purchasing Agent in writing of such circumstance.
7. I/we certify that I/we will maintain required insurance during the entire course of the contract resulting from the offer contained herein and should my/our insurance lapse or be suspended, I/we shall immediately inform the Rhode Island State Purchasing Agent in writing of such circumstance.
8. I/we certify that I/we understand that falsification of any information herein or failure to notify the Rhode Island State Purchasing Agent as certified herein may be grounds for suspension, debarment and/or prosecution for fraud.
9. I/we acknowledge that the provisions and procedures set forth in this three page form apply to any contract arising from this offer.
10. I/we acknowledge that I/we understand the State's Purchasing Laws (37-2 of the General Laws of Rhode Island) and Purchasing Regulations and General Terms and Conditions available at the Rhode Island Division of Purchases Website ([www.purchasing.ri.gov](http://www.purchasing.ri.gov)) apply as the governing conditions for any contract or purchase order I/we may receive from the State of Rhode Island, including the offer contained herein.
11. **NEW REQUIREMENT\* - IMPORTANT!!!** I/we hereby acknowledge that I/we understand that effective January 1, 2011 all public works related project bids or proposals exceeding One Million Dollars (\$1,000,000), inclusive of all proposed alternates, must include a "public copy" as required by R.I. Gen. Laws § 37-2-18 and the "Rules, Regulations and General Conditions of Purchases". It is further understood that any bid or proposal in excess of One million Dollars (\$1,000,000) which does not include a copy for public inspection shall be deemed to be non-responsive.

**RIDOT Highway and Bridge Public Works related projects utilizing Quest Lite program only** - Effective immediately, submission to the Division of Purchases of a duplicate original of a vendor's Quest Lite compatible electronic copy on a readable compact disk shall satisfy the statutory "public copy" requirements. Quest Lite software is defined in the Division of Purchases "Rules, Regulations and General Conditions of Purchases" §12.102.05 (Preparation of Proposal), as adopted on December 15, 2010 and January 11, 2011.

For further information, please see R.I. Gen. Laws § 37-2-18 and specific instructions at [www.purchasing.ri.gov](http://www.purchasing.ri.gov).

IF YOU HAVE ANSWERED "YES" TO QUESTIONS #1-2 OR IF YOU ARE UNABLE TO CERTIFY YES TO ITEMS #3-11 OF THE FOREGOING, PROVIDE DETAILS/EXPLANATION BELOW AND/OR IN AN ATTACHED STATEMENT. INCOMPLETE CERTIFICATION FORMS SHALL BE GROUNDS FOR DISQUALIFICATION OF OFFER.

Signature below commits vendor to the attached offer and certifies (1) that the offer has taken into account all solicitation amendments, (2) that the above statements and information are accurate and that vendor understands and has complied with the requirements set forth herein. When delivering offers in person to One Capitol Hill, vendors are advised to allow at least one hour additional time for clearance through security checkpoints.

  
\_\_\_\_\_  
Vendor's Signature (Person authorized to enter into contracts; signature must be in ink.)

Date 4/21/11

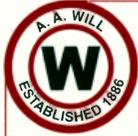
JOHN MCNAMARA Vice President  
\_\_\_\_\_  
Print Name and Title of company official signing offer  
Revised: 3/21/11



**Qualifications/Technical Proposal  
Replacement of the Laurel Avenue Br. No. 397, Conventry, RI  
Bid #7448315**

A.A. Will Corporation  
Maguire Group Inc.

# A.A. WILL CORPORATION



GENERAL CONTRACTOR

April 22, 2011

Rhode Island Department of Administration (RIDOT)  
Division of Purchases (2<sup>nd</sup> Floor)  
One Capitol Hill  
Providence, RI 0208-5855

Re: Qualifications/Technical Proposal  
Replacement of the Laurel Avenue Bridge No. 397, Coventry, RI  
Bid # 7448315  
**Letter of Submittal**

Dear Sir or Madam,

A.A. Will Corporation respectfully submits this Qualifications/Technical Proposal for your review. Together with Maguire Group Inc., we have formed a Design/Build Team to provide RIDOT with complete Design/Build Services for the replacement of the Laurel Avenue Bridge No. 397 in Coventry, Rhode Island. A.A. Will and Maguire have entered into a teaming agreement, whereby Maguire will serve as Lead Designer to A.A. Will as a subcontractor. A.A. Will is the Lead Contractor responsible for guaranteeing the delivery of the project on time and on budget in accordance with the contract provisions.

*Design / Build Team / Company Structure*

**A.A. Will Corporation (AAW)**

Lead Contractor

*Local, family-operated firm*

**Maguire Group Inc. (MGI)**

Lead Designer

*Local privately held firm*

The D/B Team will also include the following subcontractors to meet the required DBE goal:

*Design Subcontractors*

Lamson Engineering Corp.

Narragansett Engineering Inc.

Geotechnical Design MBE/DBE

Survey Services MBE/DBE

*Construction Subcontractors*

Atlantic Bridge & Engineering

Schedule Dynamics Inc.

D&M Concrete Floor Co., Inc.

F.C. Construction Corp.

M.O.N. Landscaping, Inc.

F&I Precast Conc. Beams

Scheduling Services

F&I CIP Concrete

F&I Curb & Concrete

F&I Landscaping

MBE/DBE

Pending Certification

MBE/DBE

MBE/DBE

MBE/DBE

I will be the **D/B Team Principal Contact** for the life of the project and my contact information is as follows:

Mr. John McNamara, Vice President  
A.A. Will Corporation  
145 Island Street  
Stoughton, MA 02072  
P: 781.341.4800  
F: 781.341.4404  
E-mail: [jmc@aawill.com](mailto:jmc@aawill.com)

Surety and bond information is included with the Required Forms. The D/B Team organizational information, including descriptions and organization chart, is included within our Qualifications/Technical Proposal.

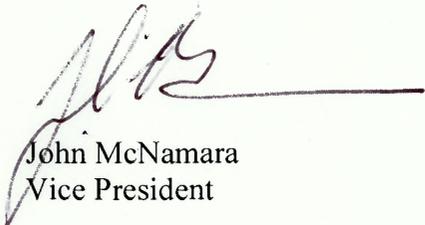
This letter is to serve as the Proposal Certification that A.A. Will Corporation will:

- (a) confirm in its proposal that the above-referenced information remains true and accurate, or, if any such information is no longer true or accurate, then
- (b) provide updated information that is true and accurate.

This letter further serves as receipt of the complete RFP.

We look forward to the opportunity to provide RI Department of Transportation a cost-effective and efficient design and construction proposal. Should you have any questions or require additional information, please do not hesitate to contact me.

Sincerely,  
A.A. Will Corporation



John McNamara  
Vice President

### 4.2a RESPONDENT'S QUALIFICATIONS

A.A. Will Corporation (AAW) and Maguire Group Inc. (MGI) have formed a Design/Build Team (DB Team) to provide the Rhode Island Department of Transportation (RIDOT) a full-service, cost-effective and efficient design and construction proposal for Laurel Avenue Bridge, located in Coventry, RI. Both firms have a long history of working on public as well as private infrastructure projects and the information presented below addresses how well this DB Team meets the specific evaluation criteria for this solicitation.

#### INTRODUCTION TO THE TEAM



AAW will manage the overall design and construction as well as self-perform the majority of construction work. AAW has 125 years of experience with heavy civil infrastructure construction projects focusing on general site work, utilities, roads and bridges. AAW excels at successfully executing challenging, high-profile, and fast-tracked projects with aggressive timelines and is currently working on and recently completed several important infrastructure projects in New England. Throughout five generations, AAW has demonstrated its commitment to public infrastructure projects for both public and private clients alike.

In 1886, AAW was founded in Milton, MA by Alexander A. Will, who utilized teams of horses and wagons to move earth and rock on public works projects. AAW has many notable achievements, including offloading ballast from merchant ships in Boston and Quincy shipyards during World War II, the original infrastructure package for the South Shore Plaza, infrastructure for the Rhode Island Hospital Bridge Building, Providence, RI, and the infrastructure for Gillette Stadium, home of the New England Patriots. AAW historically self-performs in excess of 85% of the work on its projects

and firmly believes in controlling all aspects of a project through self-performance.

AAW intends to execute this work from its Stoughton, MA headquarters, which is home to its office staff of 15 professionals including civil engineers, project managers, estimators and superintendents. The office is located on 15 acres of land that serves as the base of operations for the company-owned equipment fleet, and from which a union workforce of 150 operating engineers, carpenters, pile drivers, and laborers is directed.



MGI will be the lead designer for this project. MGI is a full-service transportation engineering firm with offices throughout the Eastern Seaboard. All design work for this project will be performed through MGI's Providence, RI office.

Throughout its history, MGI has played an important role in the planning, design, and construction of the infrastructure used daily by millions of Americans. MGI has provided over 70 years of services to its clients and in the last five years has planned and designed over 1,800 projects. Success in the highly competitive field of design consulting is best achieved through consistent excellence, professional dedication, and services that merit client trust. MGI's most precious assets are the capabilities of its staff and the confidence of its clients.

MGI's **Transportation Division** has made an indelible imprint on the transportation infrastructure of the Northeast and Mid-Atlantic. From the design of portions of Boston's massive Central Artery Project (e.g., the intersection of I-90 and I-93) or Providence's "Iway" reconfiguration of Interstates 195 and 95, to construction management of Pennsylvania's I-99 and literally hundreds of smaller roadway and bridge projects, MGI has played a

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

significant role in the development of the nation's transportation network so essential to American prosperity and our way of life. MGI is proud that it has cultivated a reputation as the firm its clients think of first. That reputation is a clear manifestation of the quality of MGI's work, the experience and expertise of its staff, the creativity of its transportation solutions, and responsiveness to clients' needs.

MGI has participated in a number of design-build ventures. The success of these partnerships stems directly from the fact that MGI understands the DB process. The designer's role is to work in a collaborative relationship with the builder and the owner. These three entities become a team with a common goal. It is the commitment to the goal that results in the ultimate success of the project. For them, MGI represents a single source of comprehensive architectural, engineering and planning expertise that dramatically simplifies procurement, coordination and control – a benefit that is particularly important in developing complex fast-track projects.

Additional team members include:

**LAMSON ENGINEERING CORP. (LEC)** will provide geotechnical design and field inspection and exploration for the DB team. LEC is a 100% minority owned MBE/DBE firm providing services in geotechnical, structural, and civil engineering for public agencies and private clients. The company has provided services as a prime consultant to MassDOT – Highway Division on their bridge replacement and rehabilitation projects from studies, preliminary design through final design to construction phases. The company has also provided services on projects for other public agencies including MBTA, Mass. Port Authority, Amtrak, and RIDOT.

**NARRAGANSETT ENGINEERING, INC. (NEI)** will provide survey services for the project.

NEI was established and incorporated in Rhode Island in 1985. NEI's main concentration includes civil and environmental engineering, and land surveying. In addition, NEI provides services such as structural design and site planning to contractors, homeowners and others. Its staff is trained in all aspects of field surveying as well as CAD/computer design systems, so the firm can work on all phases of the projects providing diversity for the staff and also making project management more efficient. NEI uses state-of-the-art computer programs and field equipment (Autocad LDD, GPS systems, Robotic Transits, etc.) to achieve efficiency and reduce project cost. Such equipment helps to reduce the possibility of errors and maintain project schedules. NEI staff is equipped with all forms of current communication equipment to coordinate field and office staff.

NEI has been certified as a Rhode Island Minority Firm since 1989. Recently Kamal Hingorany and NEI won the Minority Business of the Year Award from the American Minority/ Small Business Association.

**DESIGN-BUILD PROJECT MANAGER: BILL ZANIBONI** offers 45 years of experience in the construction field including site supervision, project management, estimating, and quality control for construction of roads, bridges, utilities, deep foundations, earth support, underpinning, retaining walls, and environmental remediation projects throughout the Northeast. Mr. Zaniboni will be the point of contact with RIDOT throughout the project and will coordinate all communication among RIDOT, the design team, and the construction team.

Mr. Zaniboni will work closely with the project scheduling team to identify design and construction milestones and ensure that

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

the team is positioned to meet each of them. AAW has a proven track record of completing Public Work projects on or ahead of schedule. Specifically, in 2010, Mr. Zaniboni was Project Manager for two MassDOT-Highway Division projects, Dighton-Taunton and Hanover-Hanson, that were completed 5 and 25 months ahead of schedule, respectively, while achieving perfect scores of 100% for AAW's overall record of performance from MassDOT – Highway Division.

This success is accomplished by careful and diligent pre-planning and scheduling. Integral to this process is the development of a CPM schedule using the latest *Primavera* scheduling software (P6). A benefit to creating a detailed CPM schedule is that alternative methods of construction or re-sequencing of certain activities can be instantly entered into the schedule and analyzed in a “what if” scenario. This feature allows our Team to understand and act appropriately on different options available through design and construction of the project. Furthermore, impacts to the project schedule and milestones by adding crews and equipment can be instantly determined. As the work advances, Mr. Zaniboni will oversee subcontractor and supplier selections as well as the estimating and cost control process.

**DESIGN MANAGER: YIHUI PETER WU, PE PHD**, has 17 years' experience in structural engineering with primary focus on bridges. He also has extensive experience with bridge construction, management of transportation projects, seismic analysis, and retrofit for bridges. Dr. Wu has served as Project Manager for both RIDOT transportation projects and MassDOT bridge design and construction projects. He was active in the structural design for many of the bridges on the RIDOT I-195 Relocation, including the signature network arch bridge.

Dr. Wu has hands-on experience with bridge replacement DB projects. Specifically, he served as Project Manager for design review and construction oversight on behalf of the owner, MassDOT. The bridges include Route 2 over 2A in Phillipston and Cedar Avenue over Route 9 in Wellesley.

In addition to onsite experience, Dr. Wu was selected for peer review of FHWA's “Guidelines for Seismic Retrofitting of Steel Truss Highway Bridges - 2005,” as well as author/co-author 25 technical papers.

During this project, Dr. Wu will be in charge of the project design; supervise and coordinate the various design disciplines; and ensure the overall project design is in compliance with the contract documents. He will also serve as Design QC Manager by establishing a design QA/QC program for all pertinent disciplines..

**CONSTRUCTION MANAGER: MIKE RENNIE** has 18 years of experience constructing public as well as private infrastructure projects including bridges, highways, tunnels, utilities and site work. Onsite full time, he will be responsible for day-to-day project management including safety, scheduling, cost, coordination of site operations, engineering, and onsite quality control.

**GEOTECHNICAL ENGINEER. KIN LAM, PE**, has over 25 years of experience in a wide range of geotechnical, structural and civil engineering projects. He has managed and designed many projects for RIDOT and MassDOT from the preliminary design through final design to construction. Preparation of reports including geotechnical, construction plans, specifications and estimates are part of the design effort. Mr. Lam served as the geotechnical engineer for RIDOT's Amtrak Wellington Signal Bridge at the Warwick Intermodal Train

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

Station. He also served as the geotechnical engineer for three MassDOT DB projects: the CSX Bridge Bundle Project; the Lowell Bridge Bundle Project; and the Cedar Street over Route 9 in Wellesley project.

As the DB team geotechnical engineer, Mr. Lam will be responsible for the project geotechnical investigations, geotechnical data and interpretive report preparation, foundation design, geotechnical inspection, and geotechnical submittal review.

**PROJECT QUALITY CONTROL ADMINISTRATOR: DAVID B. FREEMAN, PE,** has over 30 years of experience in all aspects of highway/ roadway design for transportation projects. He serves as a key MGI project manager and principal highway engineer performing urban and rural roadway designs, traffic analysis and impact studies, environmental studies, and utility design and coordination. Currently, Mr. Freeman serves as a Senior Project Manager for the award-winning I-195 Relocation Project, also known as the Iway Project, in Providence, RI. He is responsible for supervising the preparation of the final design for this \$623-million project, including overseeing the efforts of a dozen primary subconsultants and over 30 MGI staff.

On the Laurel Avenue Project, Mr. Freeman will serve as the Project QC Administrator, responsible for the project quality system manual, QC plan in compliance with RIDOT standards, ensuring project construction and design compliance with the project QSM, QC Plan and RIDOT standards.

**CONSTRUCTION QUALITY CONTROL ADMINISTRATOR: BILL ZANIBONI, AAW** has an in-house QA/QC program to comply with RIDOT requirements. As Senior Project Manager, Mr. Zaniboni is currently managing the QA/QC program. He will oversee the QA/QC process on this project

and intends to use professionals who specialize in bridge construction quality requirements. These professionals will be held accountable for building quality into the work. Quality Assurance will include: construction inspection, independent assurance sampling and testing, material testing and material documentation.

**SAFETY MANAGER: ELIZABETH WILL** has over 20 years of experience in Corporate Risk Management and serves as AAW's Safety Manger. Ms. Will will develop a site specific health and safety plan, both inside and outside of the construction area, including maintenance of traffic traveling through and adjacent to the construction area. She will work closely with the DB Team with weekly inspections, evaluating hazards and economically viable solutions to ensure both employee and project safety in compliance with regulatory requirements.

**SCHEDULER. SCHEDULE DYNAMICS. KATHLEEN GORMAN** offers over 23 years' experience in construction management and specific extensive experience with CPM scheduling and construction sequencing on heavy civil infrastructure projects. She will provide the construction schedule for this project and ensure that the project will stay on or ahead of schedule.

**ENVIRONMENTAL MANAGER: WILLIAM G. YOUNG, PE** is a registered professional engineer with over 12 years of experience in civil/site and transportation engineering. He has been the core engineer on a wide range of federal, state, municipal, and private sector projects. Mr. Young is known throughout the industry as a provider of highly sustainable, value engineered, low-impact development solutions due to his extensive background in engineered stormwater and fast track bridge solutions. . In his capacity as Regional Engineer with Contech Construction Products, Inc. Mr. Young worked with A.A. Will on both the

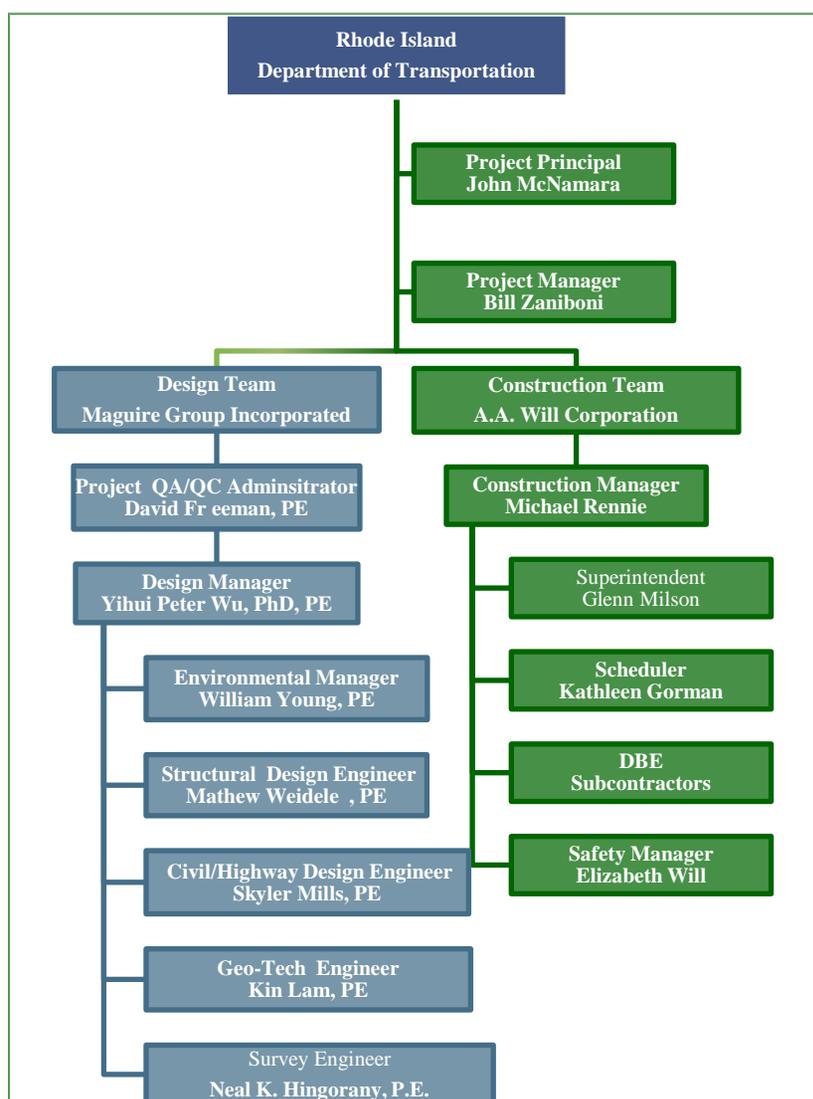
Dighton-Taunton and Hanover-Hanson MassDOT projects. The Hanover-Hanson project was actually a value engineering change proposal derived and formulated by Mr. Young, Mr. McNamara, and Mr. Zaniboni. This successful V.E.C.P. actually resulted in moving the bridge abutments completely out of the wetlands and construction the bridge with fully pre-cast components. Based on Mr. Young's mitigation strategies, no temporary or permanent wetland impacts whatsoever took place as part of this project.

On the Laurel Avenue Project, Mr. Young will be responsible for the oversight of all applicable environmental permitting as well as oversight of construction phase environmental monitoring to ensure compliance with the permits regulating the project. Mr. Young will be backed by a team of engineers and biologists/wetland scientists at Maguire with years of collective experience in hydraulics/hydrology, stormwater permitting, and environmental compliance.

The Team considers the following additional staff to be crucial to our success, although this position was not requested specifically on the RFP:

**SURVEY: NEAL K. HINGORANY P.E, PLS,** has over 30 years' experience in field surveying experience, property line surveying, survey plan preparation and reconciliation. He has served as the survey for RIDOT's Hussey Memorial Bridge project, Aquidneck Land Trust project, and RI municipal infrastructure project. He will be responsible for field surveying, topographic and existing conditions surveys, locating existing bridge structures (abutments, railings, approach, etc.), and preparation of base mapping.

**ORGANIZATIONAL CHART**



Proposed project personnel have a proven track record on similar projects. The following additional information details the expertise and capabilities of the key individuals that comprise our proposed Team.

The DB Team is a collaborative effort between AAW, MGI and RIDOT. The Project Principal will provide an organizational structure coordinated by the Project Manager (PM) who will be the primary point of contact and will coordinate all communication between RIDOT and the team. He will be supported by the project’s Design Manager (DM), Construction Manager (CM), and QC Administrator.

The DM will supervise and coordinate the individual design disciplines and ensure the overall project design is in conformance with the contract documents. He will also serve as Design QC Administrator to establish and oversee a design QA/QC program for all pertinent disciplines involved in the design of the project. This will include coordinating design submittals and communications with RIDOT for quality review and approval.

The CM will also collaborate with the PM on development of the baseline CPM schedule, and monitoring of the schedule’s critical path. Additional staff as needed will assist the PM in other areas such as QA/QC and safety.

Most importantly, the D/B team will assure that continuous interactive communication with RIDOT is maintained. We understand that the client’s staff time is limited and that an efficient method to keep the client aware of all key issues, decisions, and changes in project scope will be necessary. As a result, continuous interaction between the D/B Team and RIDOT will keep the project in motion and on-track, and will help to resolve potential problems before they become time-critical.

To assist the PM, the Project QC Administrator will develop the project *Quality Control System Manual* and *Quality Control Plan* and ensure their compliance with RIDOT standards to ensure that the project construction and design comply with the project QSM, QC plan and RIDOT Standards.

**Relevant Work Experience**

**FOXBORO STADIUM INFRASTRUCTURE**

**Client** - Foxboro Realty Associates, LLC

**Location** - Route 1, Foxboro, MA

**Completion Date** - 2004

**Contract Value** - \$23,776,557.00

Reference - Dan Krantz (508) 384-4330

**Relevance**

- ✓ Site work
- ✓ Roadway Construction
- ✓ Utility
- ✓ Bridge

**PROJECT DESCRIPTION:** Construction of 8,000LF of multi-lane roadway and parking facilities, including excavation and reclamation, pavement, drainage and roadway lighting on a 183 acre site, as well as 4 pedestrian bridges. Work included: 183 acres of clearing & grubbing, 725,000CY of excavation, 110,000CY of class A rock excavation, crushed & reprocessed on site, 50,000CY topsoil stripped, processed, rehandled & spread, 31,000LF storm drainage, 25,000LF of sewer & water pipe, 120,000T of hot mix asphalt, 80,000LF curbing, Relocation of the Neponset River, including wetlands replication, 220,000LF electric/ telecommunication conduit & 500 light fixtures, Pedestrian bridges & 7,000 SF of retaining walls.



**EMERSON PARAMOUNT CENTER**

**Client** – Emerson College

**Location** – Washington Street, Boston, MA

**Completion Date** - 2008

**Contract Value** - \$2,800,000.00

Reference – Mike Faia (617) 828-8665

**Relevance**

- ✓ Deep foundations in close proximity to structurally sensitive walls
- ✓ Utility

**PROJECT DESCRIPTION:** Reconstruction of the Paramount Theatre to create new 590 seat main theatre, 150 seat blackbox theatre, screening room, soundstage, including new residence hall for students. Construction included structural preservation of the Paramount Theatre including sensitive excavation and foundation underpinning of historic theatre in Downtown Boston. Vibration and settlement monitoring was also provided to insure integrity of the existing structure which was temporarily braced during construction.

**SPAULDING REHABILITATION REPLACEMENT FACILITY**

**Client** – Spaulding Rehabilitation Hospital

**Location** – Charlestown Navy Yard, Charlestown, MA

**Completion Date** – Phase I 2010

**Contract Value** - \$18,000,000.00

Reference – Dennis O’Connor (617) 293-4715

**Relevance**

- ✓ Historic stone masonry construction/restoration
- ✓ Site work
- ✓ Utility

**PROJECT DESCRIPTION:** Construction of new hospital facility on the Boston Harbor Water Front at the Charlestown Navy Yard. Construction included the rebuilding and extension of an existing historic granite seawall approximately 20-ft-high and 300-ft-long under Boston Harbor tidal conditions. Historic timbers were also salvaged and shipped to Mystic Seaport for use in the reconstruction and preservation of the Charles W. Morgan Whaling Ship.

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL



### CEDAR STREET OVER 2A ROUTE 9 BRIDGE REPLACEMENT D/ B

**Client:** MassDOT

**Location:** Wellesley, MA

**Year of Completion:** Ongoing

**Contract Value:** \$3.45 million

**Reference:** Walter Heller, PE, MassDOT  
Director of Accelerated Program Delivery  
(617) 973-8183

#### Relevance

- ✓ D/B Design & Construction Review/ Management

**PROJECT DESCRIPTION:** MGI was engaged by MassDOT to provide design review and construction oversight of the work performed by the DB entity for replacement of a 2-lane, 2-span girder bridge over another highway. The DB team performed design and construction services on the replacement of the bridge superstructure.

The bridge is on the MassDOT Accelerated Bridge Program (ABP, which aims to restore/repair structurally deficient bridges across Massachusetts) and is a DB project using Accelerated Bridge Construction (ABC) methods. The bridge beam will be fabricated and supported on a temporary tower with deck concrete placed offsite; self-propelled motorized transporters (SPMTs) will then be used to move the bridge to its location. A 72-hour traffic shutdown will allow for the existing bridge demolition and new bridge erection. This is

MassDOT's second "heavy-lift construction" bridge project.

MGI is responsible for review of all design submissions and construction project management for the bridge and highway work; acceptance of the final construction contract package; review of the DB team's QA/QC program and submissions; maintaining a record of submittals; and providing part-time onsite inspection services.



### I-95 SERVICE ROADS GARDEN, PINE, AND GEORGE STREET

**Client:** RIDOT

**Location:** Pawtucket, RI

**Year of Completion:** 2013

**Contract Value:** \$15 million

**Reference:** Kazem Farhoumand, PE  
Chief Engineer, RIDOT  
(401) 222-2023 Ext. 4101

#### Relevance

- ✓ Bridge Inspection, Design, & Rating
- ✓ Seismic Design
- ✓ Geotechnical Engineering
- ✓ Traffic Improvements

**PROJECT DESCRIPTION:** MGI is the lead consultant for the rehabilitation and replacement of 3 bridge structures over Route I-95 in Pawtucket. MGI has been responsible for the design of the major elements of the project and is engaged in bridge design, permitting, and geotechnical engineering. The project is a critical element of RIDOT's *Bridge 550 Pawtucket River*

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

*Bridge Replacement*; total construction cost for this project is estimated at \$115,000,000.

The project includes rehabilitation of two four-span structural steel bridges. The existing steel superstructures are to remain while the concrete deck, sidewalks, parapets, and steel bearings are to be replaced. The new deck detailing eliminates open joints over the piers to extend the service life of the structure. The existing pier substructures are to be replaced. The rehabilitation required that seismic isolation design be performed to ensure that the structure be able to withstand code required seismic affects. Also included is the replacement of a similar 4-span steel structure with a 2-span continuous butted box beam structure. A new pier and abutments were designed to accommodate a revised highway alignment and reduce the number of substructures.

MGI has been responsible for this project from the onset by developing alternatives, and preparing geotechnical reports and stormwater pollution prevention plans.



### I-195 RELOCATION, THE IWAY PROJECT

**Client:** RIDOT

**Location:** Providence, RI

**Year of Completion:** 2013

**Contract Value:** \$623 million

**Reference:** Kazem Farhoumand, PE  
Chief Engineer, RIDOT  
(401) 222-2023 Ext. 4101

#### Relevance

- ✓ A new fully directional interchange between I-195 and I-95
- ✓ 16 new bridges including Iway Bridge
- ✓ A 400-ft network arch
- ✓ 1.5 miles of new eight-lane interstate highway
- ✓ 2 miles of reconstruction and realignment of existing interstate
- ✓ 5 miles city street construction
- ✓ 22 separate drainage systems with 600 structures
- ✓ 10 miles of pipe and 25 outfalls
- ✓ 35,000 feet of utility relocation
- ✓ 80 property acquisitions
- ✓ Rigid and flexible pavement designs

**PROJECT DESCRIPTION:** MGI is the lead consultant and program manager for relocation of I-195 in Providence. MGI has been responsible for the design of the major elements of the project in addition to directing the design activities of 12 sub-consultants. MGI is engaged in all aspects of the program including road and bridge design, permitting, right-of-way procurement, and geotechnical and marine engineering.

This is Rhode Island's largest transportation project to date. The project enhances traffic operations and safety while providing significant redevelopment opportunities in Downtown Providence. It relocates a 1950's era freeway away from the center of the city and vacates over 35 acres of land that can be used to enhance the downtown business district.

The project includes 16 new bridges and several thousand linear feet of new retaining walls. The centerpiece of the project is a 400-ft-long network arch span known as the Iway Bridge. A wide range of bridge types was used, including precast concrete boxes for the short spans, trapezoidal steel boxes

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

for the longer spans, and steel I girders for the local overpasses.

MGI has been responsible for this project from the onset by developing alternatives, drafting the Environmental Impact Statement, and preparing applications for environmental permits.



### I-ROUTE 2 OVER 2A SUPERSTRUCTURE BRIDGE REPLACEMENT

**Client:** MassDOT-Highway Division

**Location:** Phillipston, MA

**Year of Completion:** Ongoing

**Contract Value:** \$3.1 million

**Reference:** Walter Heller, PE, MassDOT Director of Accelerated Program Delivery (617) 973-8183

#### Relevance

✓ D/B Design Review & Construction Oversight

**PROJECT DESCRIPTION:** MGI was engaged by MassDOT to provide design review and construction oversight of the work performed by the DB entity for the replacement of a 2-lane highway bridge superstructure over another highway. The bridge had been deemed structurally deficient because the superstructure was deteriorated and damaged from truck collisions. The DB team performed design and construction services on the replacement of the bridge superstructure, raising the profile of Rte. 2 for improved vertical clearance. To avoid future truck collisions,

the new vertical clearance was increased to 16 feet / 6 inches.

The bridge is on the MassDOT ABP and utilized Accelerated Bridge Construction (ABC) methods. ABC methods are means of rapidly building bridges by building major bridge components apart from their final location and installing them quickly using cranes, gantry systems, or self-propelled motorized transporters (SPMTs). ABC techniques minimize construction-related impacts (such as traffic congestion) by moving much of the work of building bridges out of the roadway. The bridge was constructed in just one construction season (conventional staged construction methods would likely have resulted in a 3-year project) on temporary tower supports over Rte 2A adjacent to the existing bridge, and was moved into place within an 8-day period using “heavy-lift” construction in late October 2010.

MGI was responsible for review of all design submissions and construction project management for the bridge and highway work; acceptance of the final construction contract package; review of the DB team’s QA/QC program and submissions; maintaining a record of submittals; and providing part-time on-site inspection services.



## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

<b>REPAIR/REPLACEMENT, WILBUR ROAD BRIDGE</b>
<b>Client:</b> RIDOT
<b>Location:</b> Lincoln, RI <b>Year of Completion:</b> 2011 <b>Contract Value:</b> \$2.1 million
<b>Reference</b> - Kazem Farhoumand, PE, Chief Engineer, 401-222-2053 Ext. 4100
<b>Relevance</b> ✓ Design & Construction

**PROJECT DESCRIPTION:** MGI was hired in January of 2009 to design the superstructure replacement of the Wilbur Road Bridge over Route 146 in Lincoln. The structure had low clearance and had been hit several times in the past few years. MGI's previous contract was to design repairs and pier and abutment modifications to raise the bridge. When the bridge was hit again, the decision was made to raise and replace the superstructure.

The replacement is steel beam construction with a composite deck. The joints in the deck are now over the backwalls at the abutments and the deck is continuous over the pier. The bearings will be replaced with elastomeric bearings. Design was completed using the AASHTO LRFD Bridge Design Specifications and RIDOT LRFD Bridge Design Manual.



### 4.2b Technical Criteria

The AAW/MGI DB Team understands the scope of work includes dismantling the existing north abutment, constructing new foundation behind the existing abutment, new stub abutment supported by deep foundation, new return walls, and a new superstructure to cross the river, reconstructing the existing splash pad in the river bed, constructing new spillway walls on the reconstructed splash pad, constructing a stone façade on the face of the new spillway walls, reconstructing/making repairs to upstream flood damaged stone walls as required to complete the bridge replacement, and constructing roadway approach work and repaving the bridge at Laurel Avenue and related task as necessary. The new bridge will maintain all dimensions of existing hydraulic opening as shown on the existing bridge; the curb-to-curb roadway width on the bridge will be increased to 32.15 feet. It is also required that construction activity-induced vibration will be limited, and the downstream river bank will be protected from flood damage during construction duration.

Based upon the project criteria, available data and information, the DB team proposes the following concept design:

1. As shown on the attached project Highway Plan and Profile, and Bridge Longitudinal Section Plan, the proposed bridge will have a 95-ft span, which is longer than the existing bridge. To maintain the required hydraulic opening and accommodate the spillway wall and stone façade requirements, bridge depth is increased to accommodate the RIDOT bridge design requirements with a longer span. The Roadway profile is raised up by approximately 6 inches to accommodate the bridge depth increase.
2. The proposed bridge, as shown on the attached Project Plan, is a pre-cast concrete spread box beam span, with 7

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

AASHTO Type B III-36 beams to provide 32.15 feet curb to curb bridge width spaced at 6 feet, 4 feet sidewalk on each side. The out to out bridge width is 42.15 feet. The spread box pre-cast concrete beam provides an advantage because of the pre-cast elements' fabrication duration and schedule, better quality control, and easy access for required utility support. The existing gas main and requested future water main, sewer line, Verizon conduit utility support are provided in utility bays. To accommodate future utility installation, pre-cast concrete panel approach slab will be considered for design.

3. As shown on attached plan of the proposed foundation, micropile is proposed to support the stub abutment. The proposed micropile foundation consists of 2 rows of piles each of 10" in diameter without the need for rock sockets. The pile is to be reinforced with one reinforcing bar and is cased with steel casing for the top portion of the pile to provide for lateral rigidity of the pile. Micropile foundation is also proposed to support the return wall as shown in the plan.
4. As shown on the attached Longitudinal Section and Splash Pad Plan, the splash pad and spillway wall will be a integral reinforcement concrete structure under the bridge footprint. The south stone spillway wall from bridge to upstream will be repaired and reset splash. North side stone spillway wall will be re-pointed. The proposed concrete splash pad section is designed to be 3 feet thick including approximately 6 inches of wearing surface above reinforcement. The existing splash pad, which is broken off approximately 8 to 10-ft downstream of the base of the spillway, appears to be at least 2 feet thick, as the flow drop is approximately 18 inches over the downstream broken edge. The proposed

splash pad will need to transition down near the upstream end to meet the existing channel elevations. The upstream edge of the proposed splash pad will support the downstream edge of the existing splash pad. This joint will also minimize the potential for water to seep vertically through the cold joint. The downstream edge of the splash pad will extend 5 feet beyond the downstream face of the abutments. A 3-ft-deep concrete toe will be installed to anchor the end of the slab and reduce the potential for undermining. Energy dissipators will be installed to reduce the discharge energy and mitigate erosion. A combination of concrete blocks spaced in rows and an end sill (stilling basin or variation) are common practice for this application. During construction of the proposed abutments, scour countermeasure will be installed to prevent erosion within the construction site and immediately downstream. This countermeasure will utilize the flank section design for the downstream end of the splash pad.

### 5. **Management of River Flow:**

Management of the Pawtuxet River during construction of this project is by far the most challenging and risk oriented task facing this project. This team has studied an enormous amount of available data as well as kept a close eye on the upstream reach of the river over the past several weeks to understand the lag time between rainfall events and substantial increases in flow over the spillway. There is a USGS real time river gage approximately 1.7 miles upstream of the Anthony Mills Dam. This gage has provided significant insight to the behavior of the water just upstream and over the spillway before, during, and after measurable rainfall events. From the USGS 01116000 gage we have determined that the summer, or

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

dry period base flows are generally below 60 cubic feet per second (cfs). This flow level is very manageable in regards to by-pass, however, there is a much bigger problem that must be dealt with.

The area of impoundment above the Anthony Mills Dam to the next dam upstream is only about 42 acres in size. Given this limited area for storage, there is no “storage cushion” per say. Flow Data in the Appendix shows river flow rates from July through October for the past ten years at USGS 01116000. It should be noted that regular rainfall events can push the river flow up to around 200 cfs even during the summer months. It should further be noted that the possibility of flows above 200 cfs even during the summer and fall months MUST be taken into account. This team has determined that dropping the level of the reservoir only temporarily would provide virtually no benefit. If, for instance, you artificially lowered the level of the impoundment area behind the spillway by 4 inches at the base 60 cfs assumed flow rate, it would only take approximately 2.8 hours for the level to reach the crest of the spillway again. Also, if the level of the impoundment were lowered 4 inches prior to a 200 cfs flow rate, it would fill right back up again in approximately 50 minutes. Based on the this limited “storage cushion,” it is clearly evident that the water management system must be redundant, easily scalable, and have a provision for placement of emergency flow measures to protect the splash pad and spillway wall work areas in less than 24 hours.

Based on our observations and the flow data obtained and analyzed, we have discounted the following possible solutions as being infeasible and/or economically impractical:

- Blocking or adding vertical height to a portion of the existing spillway and allowing the water to proceed through only a fraction of the spillway width (this option poses an exorbitant amount of risk from several factors, the first and foremost being the safety of the laborers below)
- Utilizing any of the existing Anthony Mills flow control structures. (It appears based on available information that the control structure in the spillway was last opened in 1984)
- Temporarily lowering the impoundment area behind the spillway
- Very slightly lowering the level of the Flat River Reservoir (2 dams upstream) and/or Tiogue Lake prior to construction of the splash pad and channel walls

There appears to be about a 24 hour lag (or time of concentration) from the peak intensity of a rainfall event to the rate of maximum flow over the Anthony Mills spillway. This preparation timeframe was duly noted and incorporated into the overall river management scheme.

Based on the above noted background information, this team has decided that it is most desirable to work in the dry flows of less than approximately 60 cfs. This, in our opinion, will be most easily accomplished by utilizing twin 24” diameter fusion welded HDPE siphons. The drawings that follow depict this regime.

Preliminary design of these siphons yields a flow capacity in the neighborhood of 40 cfs/24” diameter siphon. Fusion welded HDPE is preferred by this team due to the elimination of joints throughout the siphon system. We feel that even the highest quality restrained joints on either PVC or

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

ductile iron pipe would be extremely prone to rupture if bumped by a piece of machinery or construction debris. Any air that rushed into siphon would immediately disable it, and it would have to be very quickly re-primed. For flows between 60 and 200 cfs, this team intends to utilize either additional siphons in series, or have readily available high capacity pumps, such as those manufactured by Crisafalli. These pumps can accommodate up to 18,000 gallons per minute (gpm) or 40 cfs each. With this in mind, this team will be prepared to reach a 200 cfs bypassing capacity while keeping water from running over the spillway.

The siphons and pumps will discharge via a level spreader type piping arrangement where the discharge pipes will be extended across the river just below the limit of work. The purpose of this is to spread highly concentrated river flow back out over the width of the entire channel prior to releasing it. This will drastically reduce the amount of temporary erosion control measures required downstream of the work zone. The discharge pipes will contain an opening on the top capable of discharging the full flow required by them. This water will “fountain out” if you will over *Tensar Triton* crushed stone filled geomattresses. Triton geomattresses have been a proven highly flexible scour/revetment systems which have been used in river and coastal applications for the better part of 30 years. These mattresses are highly flexible in nature and can be drug into a work zone, and drug out again with relative ease. These mattresses are essentially flat 12” thick gabion baskets which can conform to uneven terrain. Filled with typical 1-1/2” minus crushed stone, they provide superior scour protection and will be placed directly beneath the level spreading pipes.

For events which exceed 200 cfs, and they will happen, this team intends to fully protect the work area and any other areas

which have not been finished with their permanent scour control measures. Referring back to the 24 hour lag time referred to earlier, it is anticipated that this will be coupled with the intensely close monitoring of the weather forecast throughout the duration of splash pad and channel wall work. The anticipated depth of a rainfall event can usually be accurately predicted up to three days in advance. This coupled with base flows already present combined with historical flow data as a function of historical rainfall data will allow this team to begin pulling “major event” or greater than 200 cfs flow countermeasures into the exposed areas.

The work areas will be protected from these >200 cfs events by using additional *Tensar Triton* geomattresses. These mattresses are extremely massive and incredibly strong due to the extruded polypropylene geogrid that makes up their shell. The drawings that follow depict this >200 cfs scour/worksites protection scheme.

The intent is to layer the mattress over one another from downstream to upstream across the exposed work site very similar to how one would shingle a roof. This layering pattern will indeed allow the work/unstabilized zones to get wet, however, the mats will be layered, underlain with integrally attached geotextiles and filled with appropriately graded crushed stone to fully dissipate the energy which could cause scour of the work site.

A.A. Will has anticipated a three month schedule working in the river sometime between July and October of 2012. Based on historical flow data during this timeframe, the splash pad and channel wall reconstruction could proceed very quickly with only minor interruptions. On the other hand, if we get a summer like 2008, there could be significant weather delays associated with the completion of the “in river” work. This team encourages you to review the historic river Flow Data in the

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

Appendix and re-read this section of our proposal again. It is our sincere hope that you will agree with the proactive and robust river management scheme presented.

6. The existing gas main will be restored and supported in the utility bay between the pre-cast concrete box beams. Inserts placed in the cast-in-place concrete deck will be provided to gas main support system designed by the utility company.
7. With the “No adverse effects” determination as indicated in the contract documents, the stone wall reconstruction will be done in accordance with the Rhode Island State Historic Preservation Officer (RI SHPO) through the RIDOT Culture Resource Unit. During the construction, the DB team will report any unforeseen condition(s) which may change the determination, notify RIDOT’s Project manager, and comply with RIDOT directive on whether the RI SHPO should be consulted.
8. It is anticipated that the proposed project will have minimal impact on the wetlands. The MGI/AAW Team does not plan to disturb any bordering vegetated wetlands and furthermore, there will be no draining of the water behind the dam which could impact vegetated wetlands further upstream. The base flow of the Pawtuxet River over the spill way will be *continuously monitored and mimicked* via the use of a fully adjustable temporary siphoning system that will be designed and implemented as part of this project. Base flows will be monitored via the real time USGS 01116000 (South Branch Pawtuxet River at Washington, RI) river gage. MGI and AAW will prepare a siphon system hydraulic design/rating curves, and fabrication procedure which will be presented to all regulatory agencies and refined as part of the

design and permitting process. During construction, the D/B team will monitor river heights and adjust the siphon systems in conjunction with real time data provided by the USGS gage. The siphons will be designed with air releases so that if the river level goes below the permitted limit by RIDEM and USACE, one of the siphons will drop off line. The levels and flows will be evaluated the following day, and siphons will be primed and brought on line, or dropped off line accordingly.

9. To obtain project permits, design plans and project descriptions will be prepared with applications submitted to RIDEM for approval under the Freshwater Wetlands Act and RI Pollutant Discharge Elimination System (RIPDES) program, and obtain approval for all work prior to initiating construction.

It is anticipated the project will only require an Army Corps Programmatic General Permit. The RIDEM Freshwater Wetlands Application is a joint State/Federal application form which, in addition to the Department of Environmental Management, will be processed by the U.S. Army Corps of Engineers (USACE) as necessary.

The application materials submitted for the wetlands permit can also be submitted to RIDEM to obtain water quality certification. Supplemental water quality, erosion and sedimentation control, and/or dewatering data may be required to obtain water quality certification for the project.

The DB team will prepare a Stormwater Pollution Prevention Plan (SWPPP). In accordance with the RI Pollution Discharge Elimination System General Permits for Storm Water Discharge Associated with Construction Activity,

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

the RIDOT SWPPP Template will be used for permit application. The DB team will conduct all inspection, amendments and all reporting requirements in compliance with the requirements of SWPPP, General Permits and RIPDES Regulations.

The DB team will perform its daily monitoring for compliance with all applicable state and federal environmental laws, regulations, and permits. Should any non-conformance items be identified during construction, the DB team will take immediate and continuous corrective action to bring it back to compliance.

### **PROPOSED PLAN FOR DESIGNING THE PROJECT**

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After receiving the NTP, the survey team will initiate the topographic and existing condition survey. The survey will include locating existing bridge structures (abutment, railing, approach), right of way, limits of work, centerline, cross section of approach, and utility location. The survey team will prepare the base mapping for design and construction.

The DB team geotechnical engineer will conduct additional boring, test pit, soil exploration to verify the soil condition, and support the geotech design and construction. The proposed foundation will be designed based on LRFD design approach. Geotechnical data and an interpretive report will be prepared based on the investigation and design, and will be submitted to RIDOT for approval.

To assist the early environmental permit application, the DB team will conduct hydraulic analysis including scour analysis based upon 200-year-flood levels. The early environmental permit application will be a milestone and is on the critical path to maintain the construction schedule.

Preliminary design for 30% project submittal will proceed with survey and geotechnical data. The 30% design plan submittal will be prepared in accordance with the RIDOT Project Development Guide, and will be submitted to RIDOT for review/approval. Draft special provisions will be included with the 30% submittal package. The geotechnical data and interpretive report will be part of the 30% submittal.

With the approval of the 30% design, the DB team will proceed with the final design. The DB team will request the “Over the Shoulder Review” for drafted individual major design element (superstructure, foundation, etc.) plan and associated specifications. The DB team will start procurement for the “Over the Shoulder Review” approved items.

PS&E plans and specifications will be prepared and submitted to RIDOT for review and approval after “Over the Shoulder Review” approved individual elements plan. The DB team subcontractor will prepare shop drawing submittals for the DB team design engineer to review and approve. Approved shop drawings will then be submitted to RIDOT. Construction procedures and material submittals will be submitted to RIDOT and the DB team design engineer for review and approval simultaneously.

To expedite the design and approval process, the DB team will work closely with RIDOT and other stakeholders and take advantage of “Over the Shoulder Review” for major design element submittals before the PS&E submittal package is finalized.

After the project reaches substantial completion, the As-Constructed Plan, Calculation Book, and Bridge Load Rating Report will be submitted to RIDOT for the project’s final approval.

### QUALITY CONTROL & QUALITY ASSURANCE

Our team will develop and implement a comprehensive quality control (QC) system, in collaboration with RIDOT, to ensure our performance maintain a high standard of excellence, efficiency, reliability and safety. Our QC system will achieve the following objectives:

1. Provide a unified approach for achieving design and construction quality.
2. Establish procedures for coordinating and ensuring consistency and high quality of work performed by all team members.
3. Ensure that all design documents are prepared in accordance with all RIDOT standard and contract document requirements.
4. Ensure that the quality of materials and workmanship of construction meet RIDOT requirements.

To achieve these objectives, we will develop a Quality System Manual (QSM) that address the QC organization and roles, document management, requirements for design quality control, and requirements for construction quality control.

David Freeman, PE will serve as the Quality Control Administrator, responsible for managing the overall coordination of the Quality Control System. He will report directly to the Project Manager and will coordinate all project issues directly with RIDOT. To assist Mr. Freeman for Quality Control will be Dr. Yihui Peter Wu, P.E. the Design Quality Control Manager and Mr. Zaniboni, the Construction Control Manager.

All documents and data, including design plans, specifications, reports, calculations, and other construction documents, will be organized by discipline and will be

controlled in accordance with our QSM. QC procedure will be established and monitored.

Project design production personnel and design quality team members have specific independent roles with respect to design quality control. The Team QSM will include procedures for quality control activities to be carried out by design production personnel including discipline coordination reviews, independent technical reviews, and constructability review.

The QC team will have the authority to stop work that is deemed unacceptable. The QC team's first priority is to ensure that the project is built in accordance with the approved contract documents and RIDOT quality standards. The QC team will not consider the potential impact to the project schedule or cost of their decision to stop or reject work. The Project QSM will be submitted to RIDOT for review and approval. Quality control plans for materials and construction will then be developed in accordance with the approved QSM, and submitted to RIDOT for review / approval.

### DESIGN QUALITY CONTROL AND ASSURANCE

Dr. Yihui P. Wu, PE, the Team's Design Manager, will also serve as Design Quality Control Manager. He will be responsible for overall management of the QA/QC program for design, implementation of the approved Design QA/QC System. As a consulting engineer, Dr. Wu has been involved in the design, management, and construction engineering for various RIDOT transportation projects. His expertise is in directing the preparation of the design, construction engineering and management of bridge projects.

The Design Quality Control Manager will report directly to the Project Manager and maintain close communication with the Project Manager and ensure that all designs meet the project requirements and all

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

designs are consistent with the overall project concept. He will perform all of the design oversight review. Detailed project reviews will be conducted to further ensure that there is consistency of materials, details and procedures throughout the project. The design quality control team will include a Design QA/QC Manager and independent senior level engineer as disciplinary QC engineer; the QC engineer will report to Design QA/QC Manager.

Prior to design submittals, the Design Quality Control Administrator will certify in writing to the Department that the submittal has undergone QC checking and Review by the designer. The Design Quality Control Manager will perform reviews.

The Design Quality Control Plan will be included as part of the project QSM. Project design production personnel and design quality team members will have specific independent roles with respect to design quality control. Our QSM will include procedures for the following quality control activities to be carried out by design production personnel including discipline coordination reviews, independent technical reviews, and constructability reviews.

Additionally, the QSM will outline procedures for the following activities:

- Review comment procedure forms.
- Review studies, reports and design
- Validate and approve use of design software
- Detail checking report forms for calculations, plans, and specs
- Verify reports, calculations, plans and specifications meet QSM and RIDOT quality standards.
- The Design QC team will ensure coordination between design disciplines has been achieved.
- QC Review Checklist

The Design QC team will ensure there are

no design omissions, no utility conflicts. They will confirm the project is constructible. Our QSM will also detail specific activities and responsibilities for review of shop drawings, request for information, notice of design change, field design changes, and design dispute resolution procedures. To track progress and status of the aforementioned activities, electronic logs will be developed and maintained through the use of a web-based collaborative software system. This system will be available 24/7 to all team participants, including RIDOT.

Milestones will be developed for implementation of interim reviews as the design progresses, in accordance with the Project QC System. The QA/QC manager will coordinate for the QC review schedule, priority and level. The timing of these reviews is critical. A senior level independent design professional completed the concept review of the proposed systems, methods and design approaches. Progress reviews of work products will be performed by independent reviewers at appropriate stages as the work progresses. At a minimum, reviews will be performed prior to all design submittals. Reviews will be performed on plans, specifications and other deliverables. All structural calculations will be reviewed, through a check of the calculations and QC review checklist, as appropriate. Design QC activities will be tracked in QC review form, signed by the QA/QC manager, approved and signed QC tracking form will be submitted to the Department as part of design submission.

### **CONSTRUCTION QUALITY CONTROL MANAGEMENT**

Mr. Rennie will serve as the Construction Quality Control Manager on the project site for the duration of construction operation. He will work with RIDOT's Resident Engineer, Material and Construction Section

## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

to coordinate construction quality issues.

Our quality control plans will be prepared specifically for the Laurel Avenue Bridge replacement project. These plans and any revisions will be submitted to RIDOT for review and approval. The function of the quality control plan is to address specific facility and field placement processes for individual categories of work (e.g., Hotmix asphalt, Portland cement). The quality control plan will ensure that adequate personnel, qualified/certified inspection and testing personnel, facilities, qualified/accredited laboratory/testing agencies, equipment and procedure are in place to achieve the specified product quality for each major work item. Nonconformance Reports (NCR) will be issued for any defective work. The NCR will identify the problem and recommend corrective action including identification of the root cause and methods to avoid a recurrence of the same problem. Protocols and procedures will be established with RIDOT materials section and inspector to ensure proper documentation and quality of all work.

### **PROPOSED APPROACH TO CONSTRUCT THE PROJECT**

AAW will perform all general civil construction tasks including traffic control, environmental protection, scour protection, site preparation, demolition, site work, excavation, support of excavation, concrete for abutment and walls, utilities, control of water, and the set-up, maintenance and removal of staging areas and access roads.

The following items will be performed by subcontractors:

Concrete flat work, splash pad, bridge deck, sidewalks etc. will be performed by D&M Concrete Flooring Co. Inc. who are also a major DBE participant. Furnishing and installation of the precast box beams will be done by Atlantic Bridge also a major DBE.

Stone masonry work, granite curbing, and remaining hardscape will be performed by FC Construction. MON will be our landscape subcontractor. All of the above Subcontractors are DBE's certified by the RIDOT. Micro piles and asphalt paving will be performed by a subcontractor to be determined at a later date as well as some fence work and other miscellaneous items.

Traffic control will be required during approach work and final paving for the bike path and the Laurel Ave intersection with Pilgrim Avenue. The access roads to neighboring facilities will remain open during all construction activities. Some temporary access roads will be created as required when construction directly impacts the tenants. Traffic control can be handled with jersey barriers and or reflective drums moved as required.

The construction area will have perimeter fencing with gates at the North and South sides of the bridge and dam area. All temporary facilities, staging, and storage areas should be within the fenced boundary. It is also anticipated that some additional off site staging and storage will be acquired to support best efforts.

### **PROJECT CONTROLS**

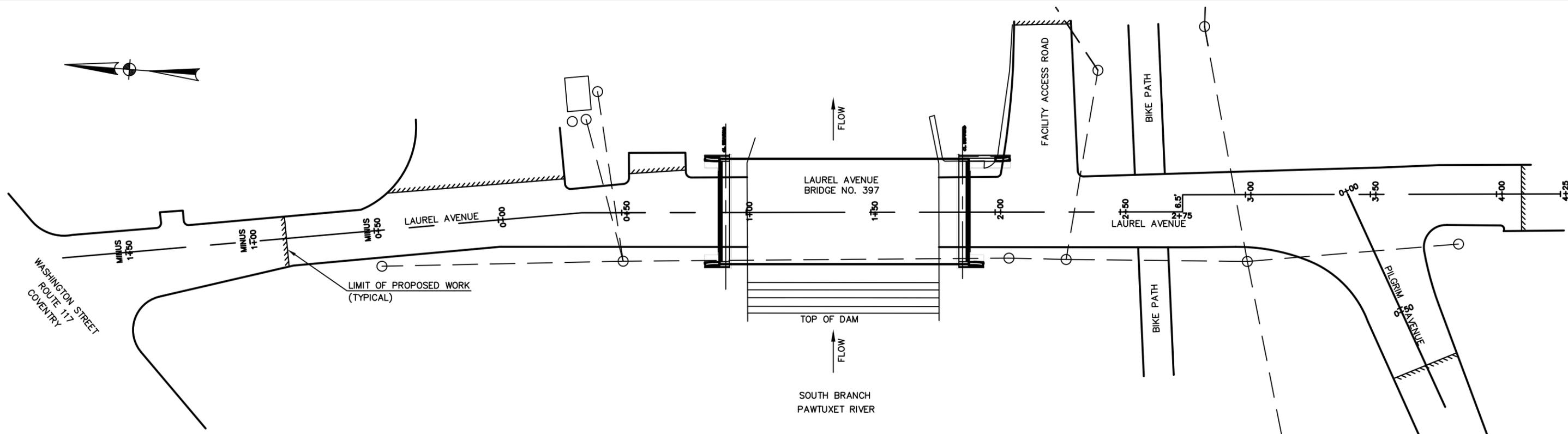
Work will begin with the stabilization of the existing conditions, particularly the South abutment slope area lost during the March 2010 storm. After the installation of the water control system and temporary outfall, the remainder of the existing bridge will be demolished and splash pad and wall construction will begin with the installation of the temporary support of excavation system.

It is during this time that the biggest risks will be faced, the key being the timely completion of the in-water work with an eye toward upcoming weather events. The

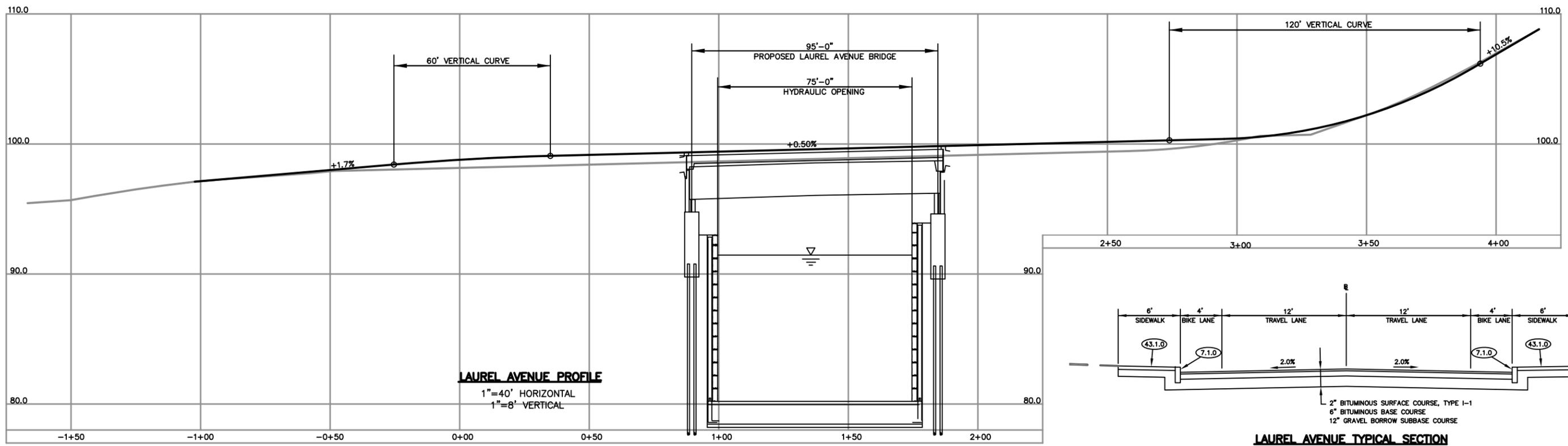
## 4.2 QUALIFICATIONS/TECHNICAL PROPOSAL

biggest challenge will be to have a plan in place to protect the work and surroundings during a storm event that increases the water flow rate. This will be accomplished by advance lowering of the water to a level that can contain a predicted storm event thus always maintaining pre construction water levels. In water work is expected to occur during two 20 day periods from the beginning of June through September 15th for the North and South portions of the splash pad and spillway walls.

Access ramps will be constructed to allow construction of the micro piles and foundation concrete which will be done concurrently with the completion of the spillway walls. The water control system will be removed after the completion of the downstream splash pad scour protection. The next phase of construction will be the placement of the box beams, bridge deck, sidewalks, and railings. The bridge will be open for use following the bituminous pavement and the final completion of approaches and landscaping will follow shortly thereafter.



**LAUREL AVENUE PLAN**  
1"=40' HORIZONTAL



**LAUREL AVENUE PROFILE**  
1"=40' HORIZONTAL  
1"=8'-VERTICAL

**LAUREL AVENUE TYPICAL SECTION**  
1"=10'

**Maguire Group Inc.**  
Architects/Engineers/Planners  
225 Chapman Street  
Providence, Rhode Island 02905



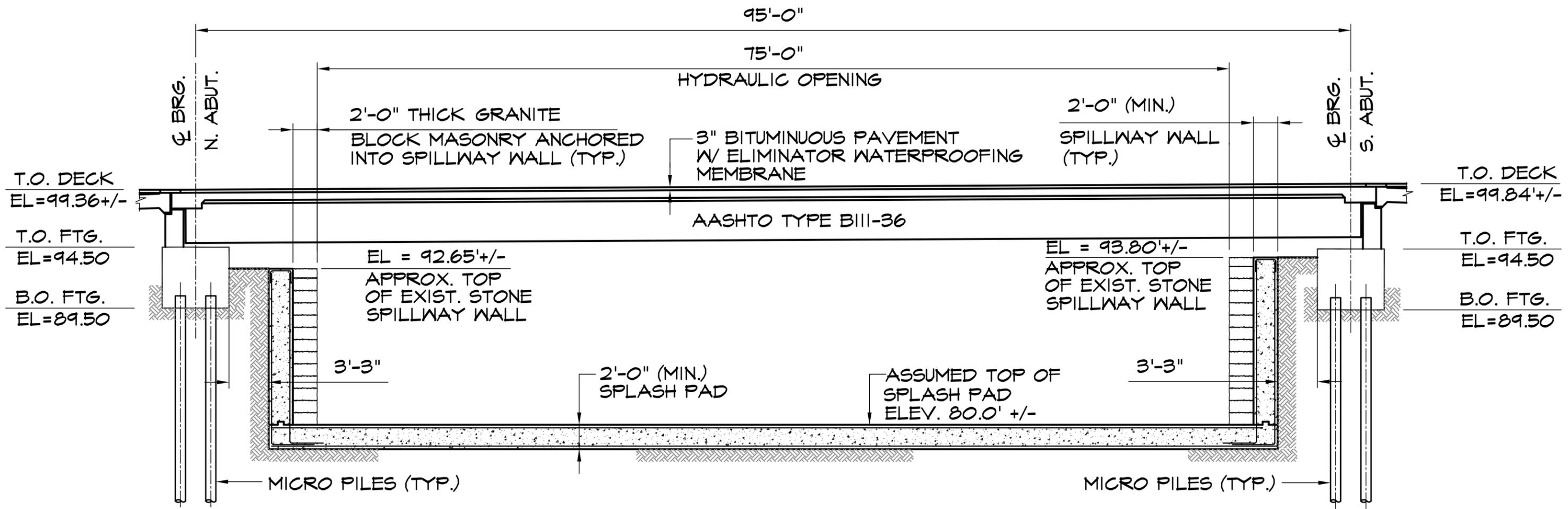
**A. A. WILL CORPORATION**  
145 Island Street  
Stoughton, Massachusetts 02072



**RHODE ISLAND**  
DEPARTMENT OF TRANSPORTATION

SCALE: AS SHOWN  
DATE: 4/18/2011  
SHEET 1 OF 5

**REPLACEMENT OF LAUREL AVENUE BRIDGE NO. 397**  
COVENTRY RHODE ISLAND  
**HIGHWAY PLAN AND PROFILE**



# LONGITUDINAL SECTION

SCALE: 1/8" = 1'-0"

**Maguire Group Inc.**  
Architects/Engineers/Planners  
225 Chapman Street  
Providence, Rhode Island 02905



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145 Island Street  
Stoughton, Massachusetts 02072



RHODE ISLAND  
DEPARTMENT OF TRANSPORTATION

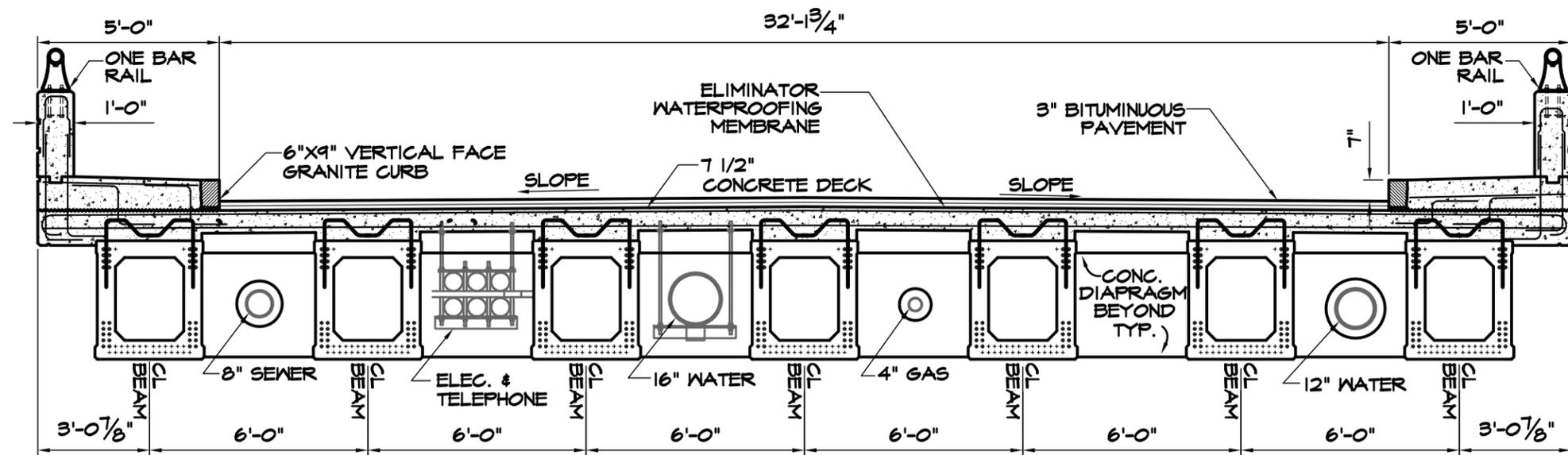
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SHEET 2 OF 5

REPLACEMENT OF LAUREL AVENUE BRIDGE  
BRIDGE NO. 397

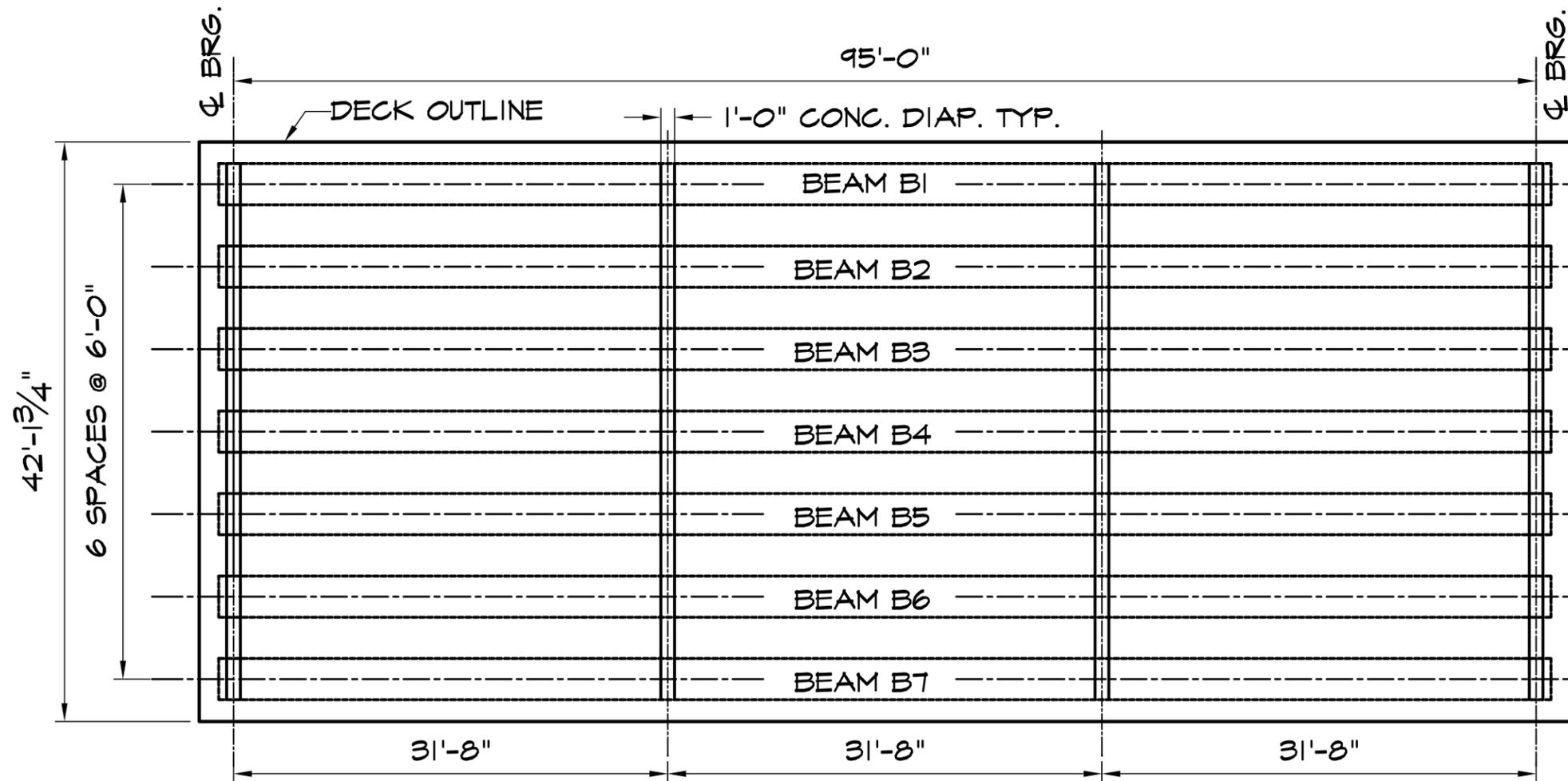
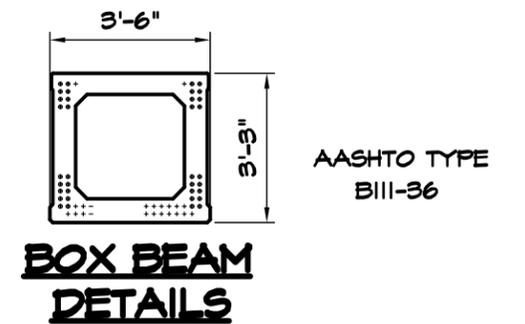
COVENTRY RHODE ISLAND

LONGITUDINAL SECTION



**BRIDGE CROSS SECTION**

SCALE 1/4" = 1'-0"



**FRAMING PLAN**

SCALE 3/32" = 1'-0"

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225 Chapman Street  
Providence, Rhode Island 02905



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Stoughton, Massachusetts 02072



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DEPARTMENT OF TRANSPORTATION

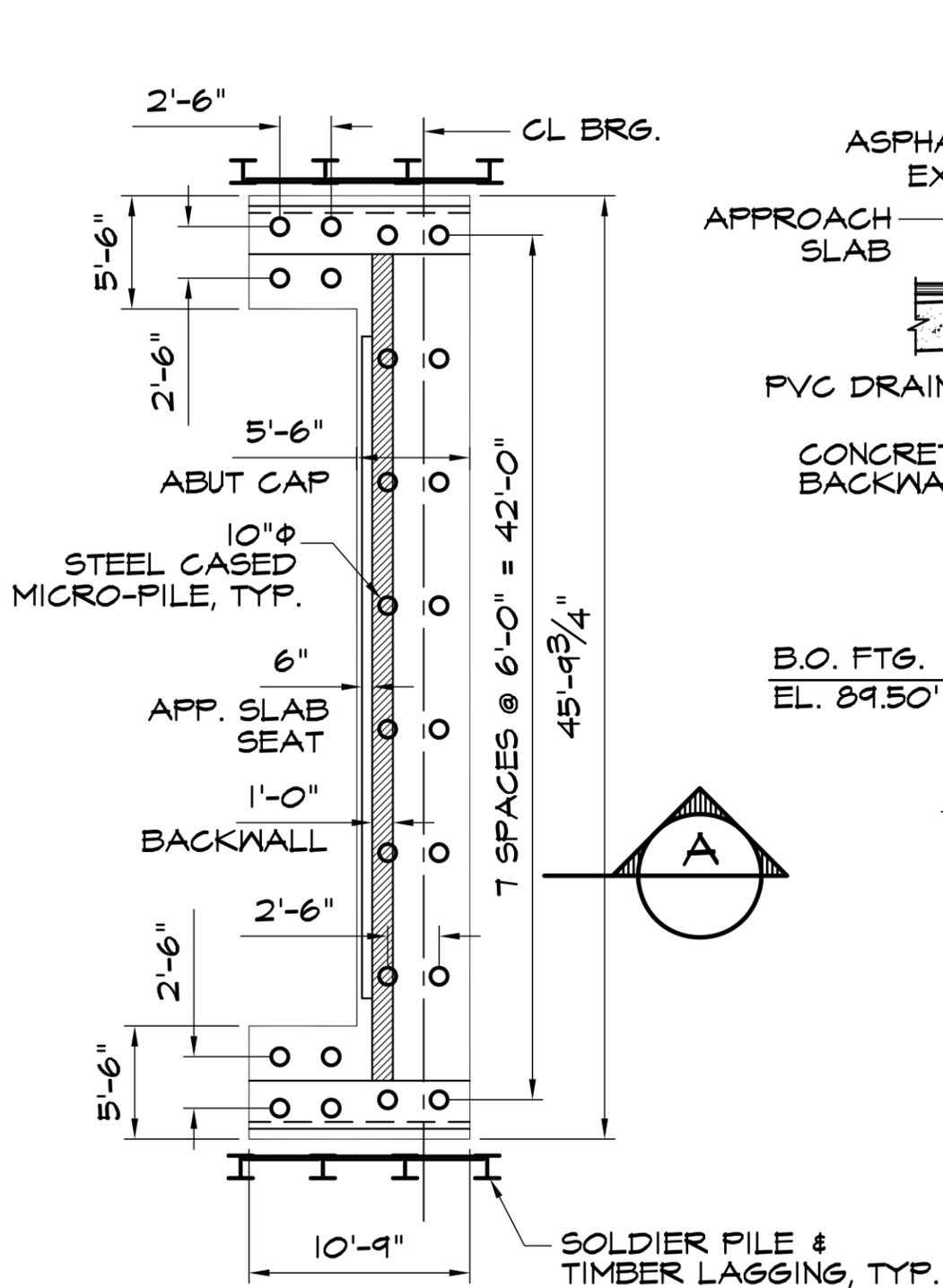
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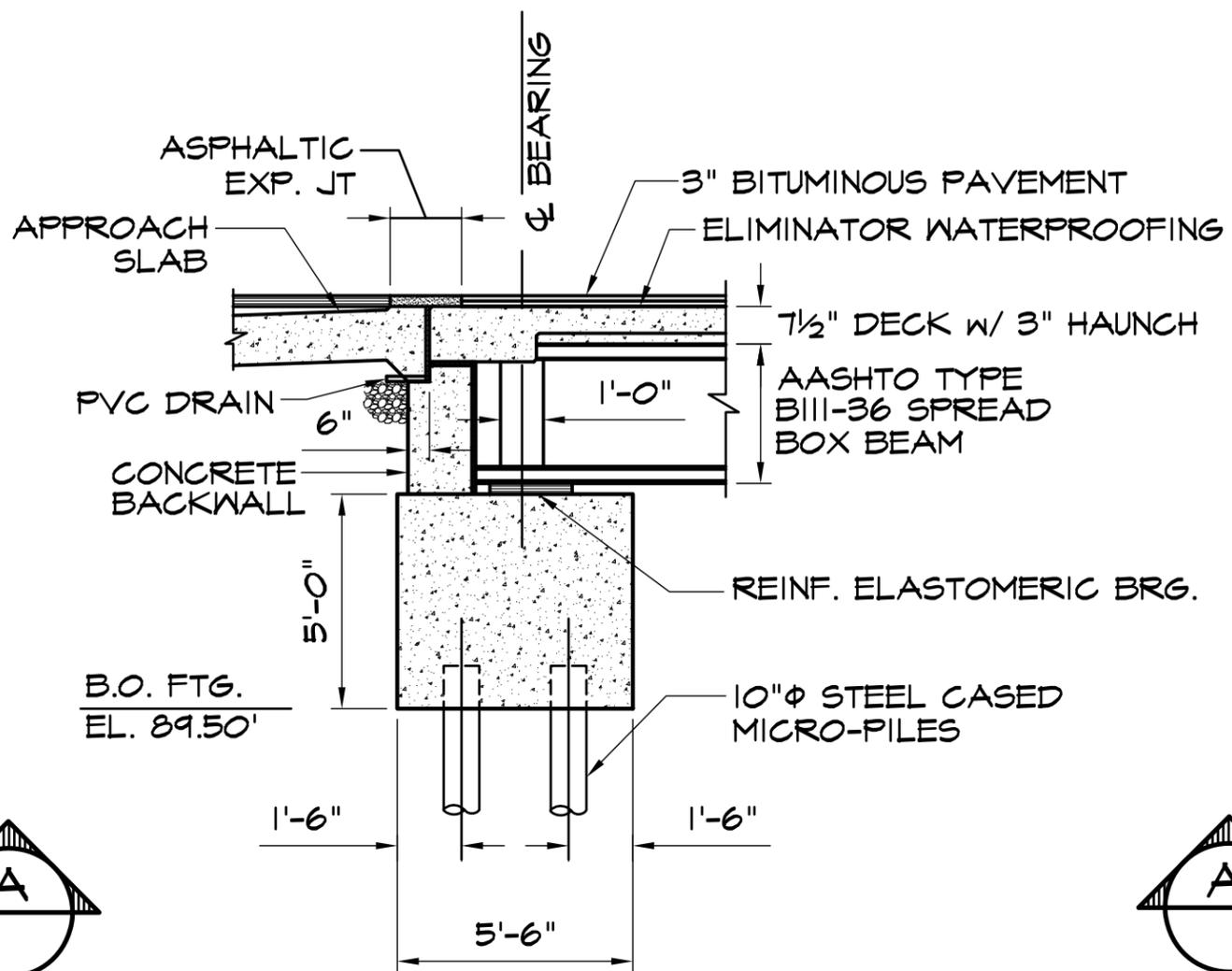
REPLACEMENT OF LAUREL AVENUE BRIDGE  
BRIDGE NO. 397

COVENTRY RHODE ISLAND

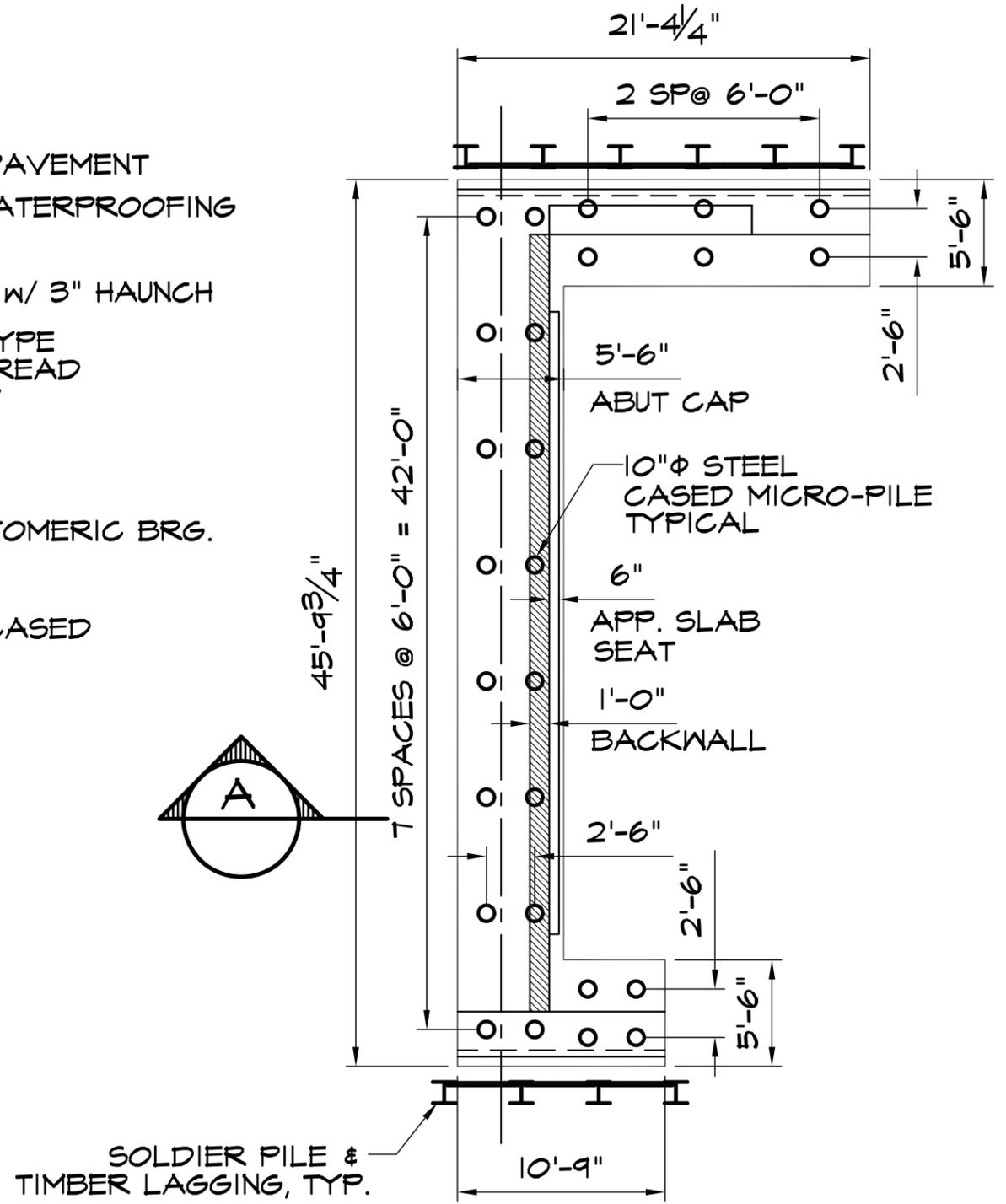
**PLAN & SECTION**



**[NORTH ABUTMENT]  
FOUNDATION PLAN  
SCALE 1/8" = 1'-0"**

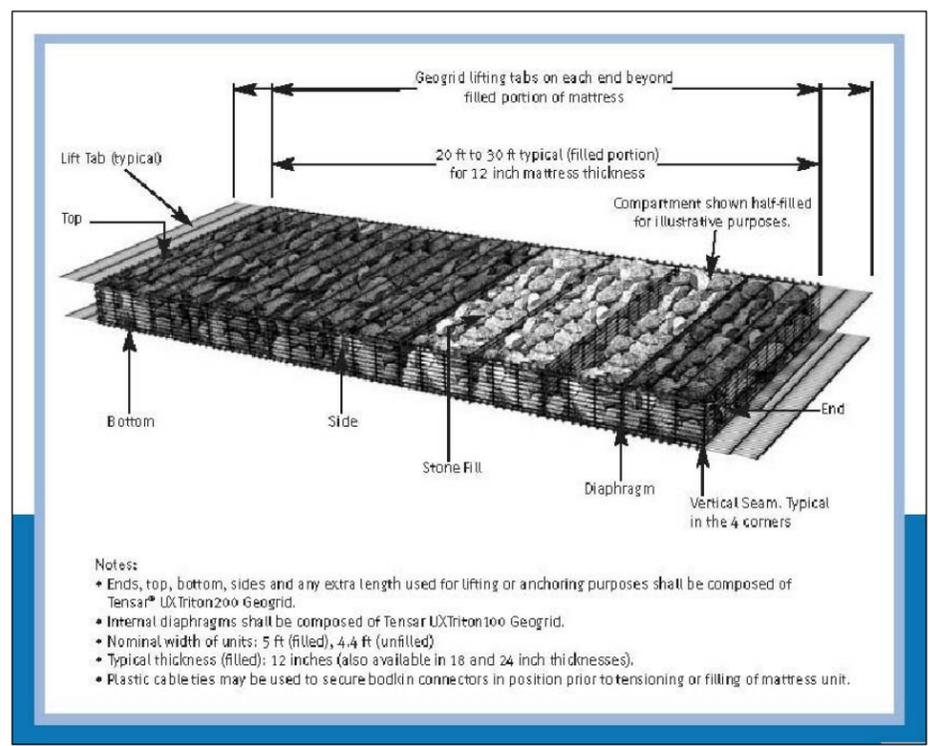
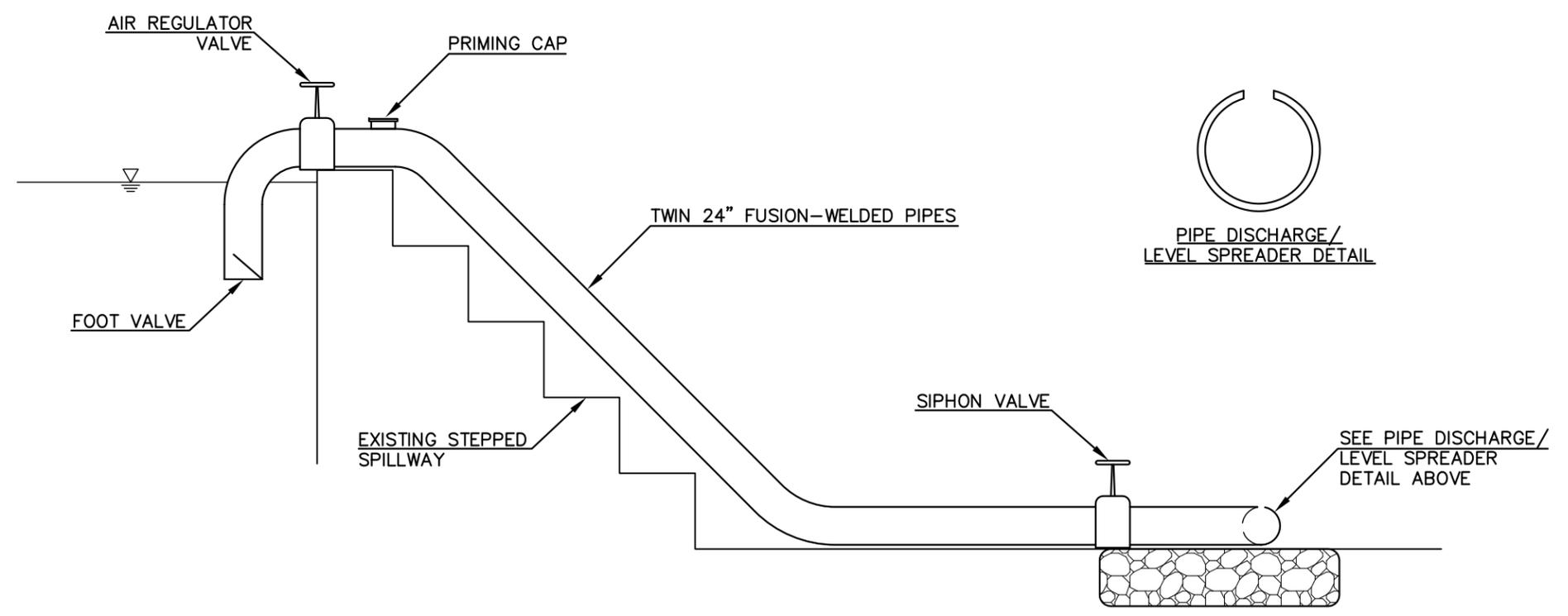
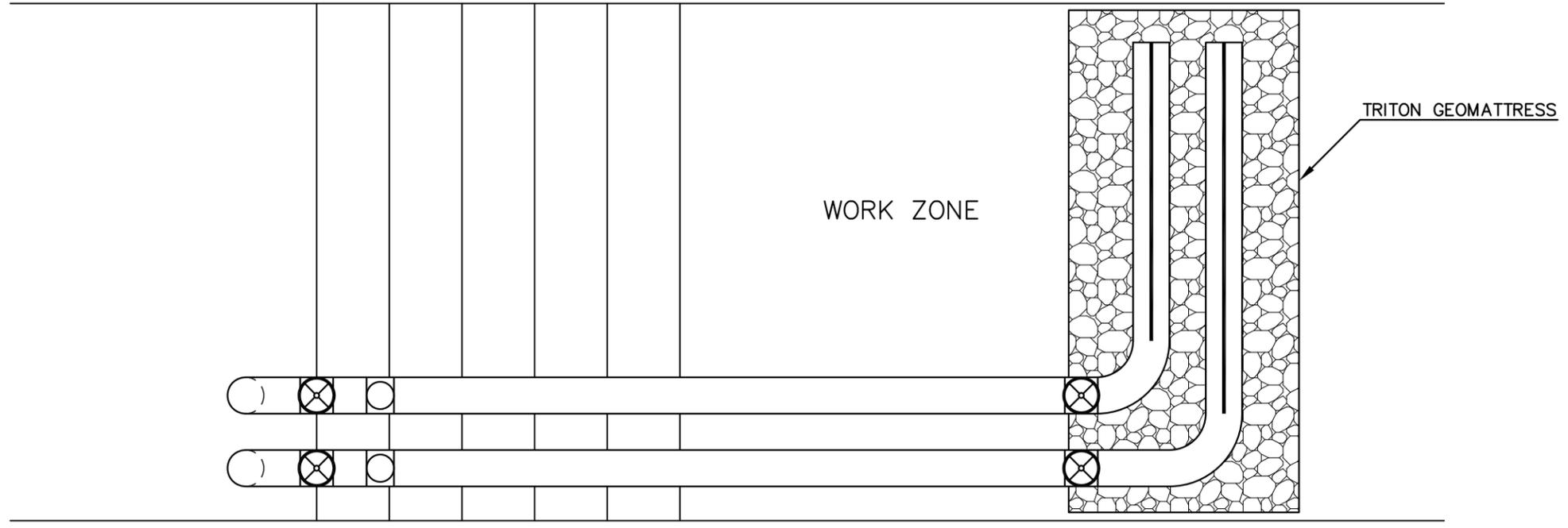


**ABUTMENT SECTION  
SCALE 1/4" = 1'-0"**



**[SOUTH ABUTMENT]  
FOUNDATION PLAN  
SCALE 1/8" = 1'-0"**





WORKING WATER MANAGEMENT (60-200 CFS)  
 ALL FLOW THROUGH SIPHONS AND/OR PUMPS - NONE OVER SPILLWAY

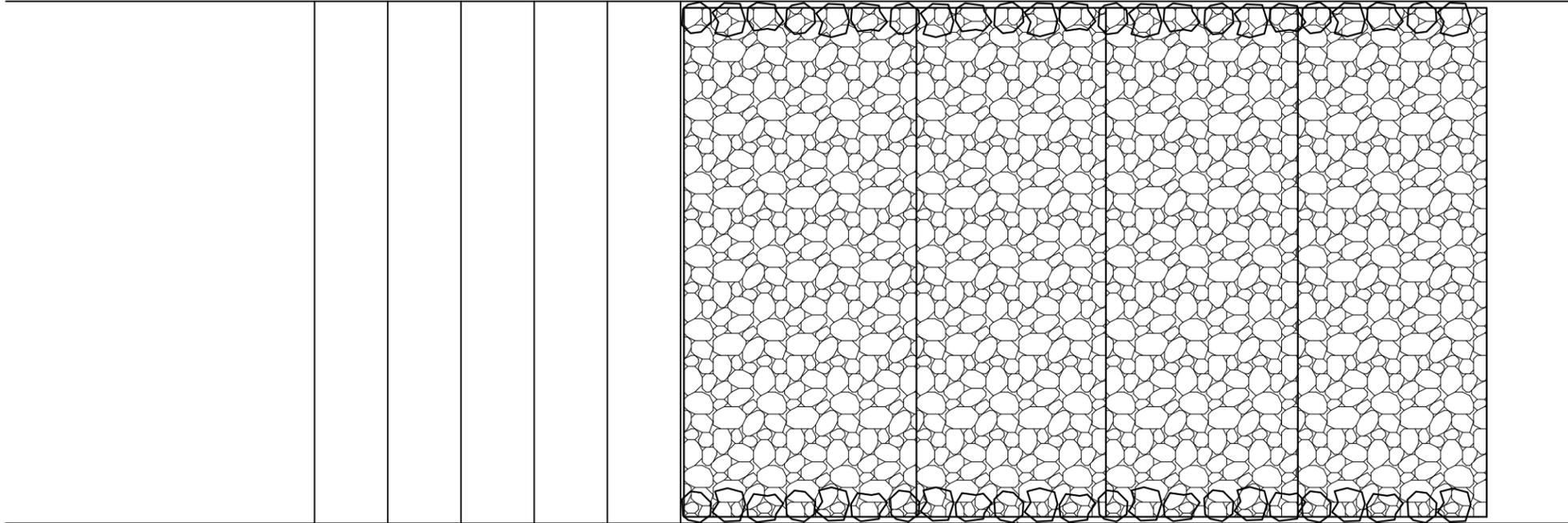
**MAGUIRE GROUP**  
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 225 Chapman Street  
 Providence, Rhode Island 02905

**A. A. WILL CORPORATION**  
 145 Island Street  
 Stoughton, Massachusetts 02072

**RHODE ISLAND**  
 DEPARTMENT OF TRANSPORTATION

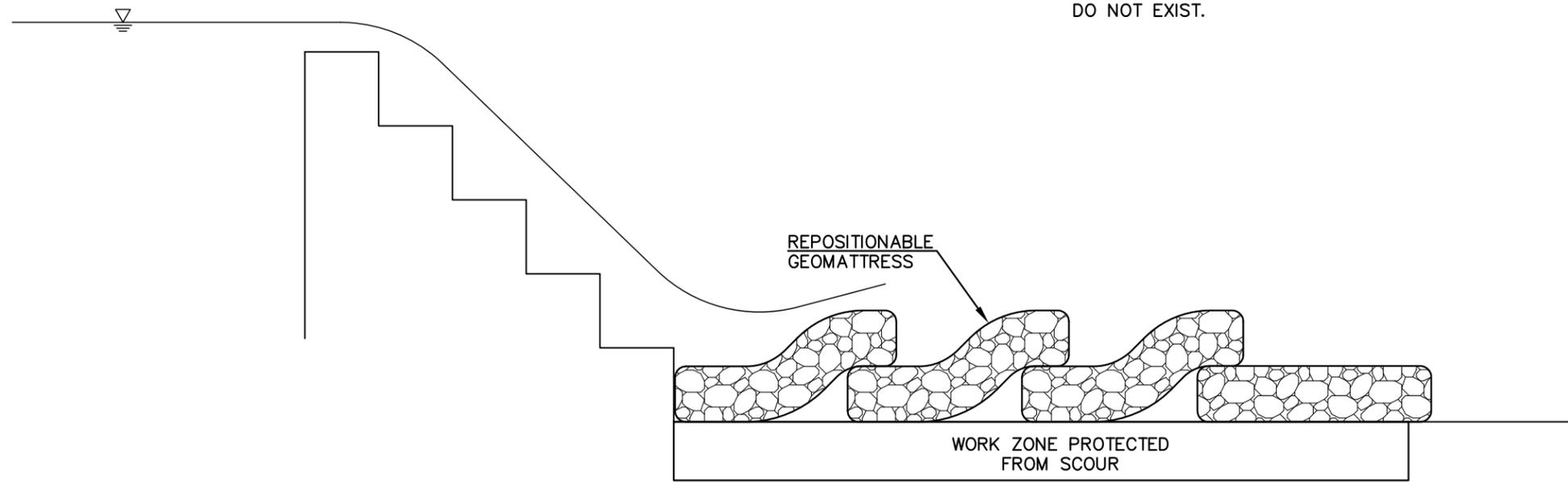
SCALE: NOT TO SCALE  
 DATE: 4/21/2011  
 SHEET OF

**REPLACEMENT OF LAUREL AVENUE BRIDGE**  
 BRIDGE NO. 397  
 COVENTRY RHODE ISLAND  
**WORKING WATER MANAGEMENT**



ADDITIONAL RIPRAP TO PROTECT EDGE OF SPILLWAY

\*NOTE: GEOMATTRESSES WILL EXTEND UP BANKS WHERE WALLS DO NOT EXIST.



EMERGENCY EVENT WATER MANAGEMENT (>200 CFS)  
FLOW OVER SPILLWAY



**MAGUIRE GROUP**  
Maguire Group Inc.  
Architects/Engineers/Planners  
225 Chapman Street  
Providence, Rhode Island 02905

**A. A. WILL CORPORATION**  
145 Island Street  
Stoughton, Massachusetts 02072

**RHODE ISLAND**  
DEPARTMENT OF TRANSPORTATION

SCALE: NOT TO SCALE

DATE: 4/21/2011  
SHEET OF

REPLACEMENT OF LAUREL AVENUE BRIDGE  
BRIDGE NO. 397

COVENTRY RHODE ISLAND  
EMERGENCY EVENT WATER MANAGEMENT

ID	Task Name	Duration	Start	Finish	Predecessors	Resource Names
1	Notice to Proceed	1 day	7/1/2011	7/1/2011		
2	Survey and Geo-Tech Investigation	21 days	7/8/2011	8/5/2011	1	MGI/Lamson/NGC
3	Historical/Environmental Permit, SWPPP, and Coordination	30 days	7/4/2011	8/12/2011	1	MGI
4	Permit Review and Approval	8 wks	8/15/2011	10/7/2011	3	RIDOT,RIDEM
5	30% Design	30 days	8/8/2011	9/16/2011	2	MGI[150%]
6	30% Submission Review & Approval	14 days	9/19/2011	10/6/2011	5	RIDOT
7	Geo-Tech Report Submittal	30 days	8/8/2011	9/16/2011	2	Lamson
8	Geo-Tech Report over the Shoulder review and Approval	14 days	9/19/2011	10/6/2011	7	RIDOT
9	Final Design - Profile/Highway	30 days	10/7/2011	11/17/2011	6	MGI
10	Final Design - Profile/Highway over the Shoulder review and Approval	14 days	11/18/2011	12/7/2011	9	RIDOT
11	Final Design - Foundation	15 days	10/7/2011	10/27/2011	6	MGI/Lamson
12	Final Design - Foundation over the Shoulder review and Approval	14 days	10/28/2011	11/16/2011	11	RIDOT
13	Final Design - Superstructure	30 days	10/7/2011	11/17/2011	6	MGI
14	Final Design - Superstructure over the Shoulder review and Approval	14 days	11/18/2011	12/7/2011	13	RIDOT
15	Final Design - Substructure	30 days	10/7/2011	11/17/2011	6	MGI
16	Final Design - Substructure over the Shoulder review and Approval	14 days	11/18/2011	12/7/2011	15	RIDOT
17	Final Design - Utilities, Coordination and Support (incl. temp. utilities)	30 days	10/7/2011	11/17/2011	6	MGI
18	Final Design - Utilities and Support over the Shoulder review and Approval	14 days	11/18/2011	12/7/2011	17	RIDOT
19	Construction Mobilization Start with over the Shoulder Review & Approval	1 day	9/19/2011	9/19/2011	5	AAWILL
20	Construction Material Procurement Start	7 days	12/8/2011	12/16/2011	10,12,14,16	
21	Procure Water Control System	21 days	11/16/2011	12/16/2011	4	AAWILL
22	Procure Minipiles	21 days	11/16/2011	12/16/2011	12	AAWILL
23	Procure Precast Beams	22 days	6/1/2012	6/30/2012		AAWILL
24	21, 22, 23 Submittal Design-Build Review	21 days	12/19/2011	1/20/2012		MGI/AAWILL
25	21, 22, 23 Submittal to RIDOT & Approval	22 days	1/23/2012	2/24/2012		RIDOT
26	Temporary Utility Relocation	42 days	1/2/2012	3/1/2012		AAWILL
27	Mobilize & Install Water Control System	23 days	6/1/2012	7/1/2012		AAWILL
28	Splash Pad & Wall Construction	44 days	7/1/2012	9/1/2012		AAWILL
29	Minipiles	33 days	8/1/2012	9/15/2012		AAWILL
30	Substructure	21 days	9/1/2012	9/30/2012		AAWILL
31	Erect Beams	6 days	9/30/2012	10/7/2012		AAWILL
32	Form & Place Bridge Deck	11 days	10/7/2012	10/21/2012		AAWILL
33	Form & Place Bridge Sidewalk	17 days	10/21/2012	11/15/2012		AAWILL
34	Roadway Approach & Utilities	21 days	9/15/2012	10/15/2012		AAWILL
35	Roadwork	21 days	10/15/2012	11/15/2012		AAWILL
36	Curb & Sidewalk	10 days	11/1/2012	11/15/2012		AAWILL
37	Guardrail and Signage	10 days	11/16/2012	11/30/2012		AAWILL
38	Open Bridge/Roadway	1 day	12/1/2012	12/1/2012		AAWILL
39	Punchlist/Landscaping	100 days	12/2/2012	5/30/2013		AAWILL
40	Construction Complete	1 day	5/31/2013	5/31/2013	20	AAWILL
41	As-constructed Plan Submittal	24 days	6/3/2013	7/4/2013	21	MGI
42	Final Bridge Design Calculation Book with As-Constructed Submittal	7 days	6/3/2013	6/11/2013	21	MGI
43	As-constructed Load Rating Calculation Book	1 day	6/3/2013	6/3/2013	21	MGI
44	As-constructed Plan, Design Calc, Rating Submittal Review and Approval	30 days	7/5/2013	8/15/2013	22,23,24	RIDOT

### 4.3 DBE and OJT Requirements

The AAW and MGI DB Team will comply with the 14% DBE participation goal and 10% for consultant qualifying work established for this project. AAW plans to utilize the following participants:

- D&M Concrete Floor Co., Inc.
- F.C. Construction
- Atlantic Bridge & Engineering
- M.O.N. Landscaping
- Schedule Dynamics

MGI's team will include Lamson Engineering Corporation and Narragansett Engineering Corporation.

Our team will further comply with the on-the-job-training (OJT) requirement and if selected for this project will provide a RIDOT-approved OJT Plan prior to award.

**RIDOT  
ON-THE-JOB TRAINING  
ACKNOWLEDGEMENT AND STATEMENT OF INTENT**

4-19-11

Date

**To:** RIDOT OJT Coordinator  
Office of Business and Community Resources  
Rhode Island Department of Transportation  
2 Capitol Hill Rm109  
Providence, RI 02903

**Project Name and Number:** Replacement of Laurel Ave Bride No. 397, Coventry RI Bid #7448315

A.A. Will Corporation (Respondent) will review the OJT requirements (Training Specification) in the contract for the above noted project. Based on these requirements, the availability of applicants within a reasonable area of recruitment, and in an effort to meet the minority and female participation goals outlined in the OJT Specification, our company will select a qualified trainee(s) and conduct training under the classification(s) identified below in accordance with the RIDOT OJT Program.

\* Selected firm must submit a copy of training plan (training classification) to be used during post qualification.

The undersigned has personally reviewed the content of each selected training classification in relation to the project scope and assures that all portions of training can be completed if initiated by the "no later than" (NLT) date indicated below.

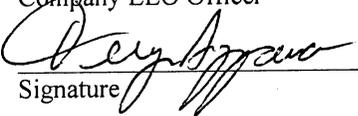
1 Selected Training Classification	2 Number of Trainees in Classification	3 Projected Start Date	4 NLT Start Date in Order to Complete training hours
1. Laborer			
2. Carpenter			
3. Operator			
4.			
5.			

**IMPORTANT:** Written justification is required to substantiate the selection of training classifications where company representation is below the minority and female participation goals specified in the contract. Compare columns (i) and (j) of the table on page 2 of 2 with 41 CFR 60-4.2, Affirmative Action Requirements.

Please provide information regarding your company's current workforce demographics in the trades listed below:

(a) Trade Classification	(b) Total Employees	(c) Female	(d) Hispanic	(e) American Indian or Alaskan Native	(f) Black	(g) Asian or Pacific Islander	(h) Total of columns (d) through (g)	(i) Minority Percentage (h) / (b)	(j) Female Percentage (c) / (b)
Constr. Supervisors	7	0	0	0	0	0	0	0	0
Constr. Foreperson	8	0	0	0	0	0	0	0	0
Carpenters	2	0	0	0	1	0	1	50	0
Equipment Mechanics	2	0	0	0	0	0	0	0	0
Equipment Operators	25	1	0	0	2	0	2	8	4
Grade Foreman Asst.									
Ironworker	1	0	0	0	0	0	0	0	0
Laborers	27	1	3	0	4	0	7	26	4
Truck Drivers	1	0	0	0	0	0	0	0	0

The authorized representative below certifies that the information proved herein is accurate and is made in good faith:

Terry Azzara  
 Company EEO Officer  
  
 Signature \_\_\_\_\_ Date: 4-20-11

Approval <input type="checkbox"/> Disapproval <input type="checkbox"/>
_____ Date: _____
RIDOT OJT Coordinator



April 19, 2011

Rhode Island Department of Transportation  
Department of Administration  
Division of Purchases  
One Capitol Hill  
Providence, RI 02908-5855

RE: **A.A. Will Corporation**  
**Design/Build Services for the Replacement of the Laurel Avenue Br. No. 397, Coventry, RI**  
**Bid No. 7448315**  
**Estimated Contract Price: \$3,400,000 +/-**

To Whom It May Concern:

This letter will serve to confirm that A.A. Will Corporation is a highly regarded and valued client of Aon Construction Services and Safeco Insurance Company of America. Safeco has enjoyed the privilege of providing A.A. Will Corporation with all of the firm's surety bonds for more than fifteen years. Aon and Safeco are both very impressed with A.A. Will's experience in all facets of heavy contracting.

With respect to A.A. Will's bonding needs, please be advised that Safeco is willing to consider providing bonding support for individual projects with contract values approaching \$100,000,000 and corresponding backlogs approaching \$200,000,000. A.A. Will Corporation has the ability to provide Performance and Payment Bonds on this project should A.A. Will Corporation be awarded a contract.

Safeco Insurance Company of America holds a Certificate of Authority as an acceptable surety, and is listed in the Department of Treasury's Listing of Approved Sureties (Department Circular 570) dated July 1, 2010. Safeco Insurance Company of America is fully licensed and authorized to transact surety business in the State of Rhode Island.

Naturally, as is customary within the surety industry, the approval of any final bonds is contingent upon a favorable underwriting review to include, but not be limited to, of the contract documents, bond forms and confirmation of complete project financing by both A.A. Will Corporation and Safeco Insurance Company of America. Please understand that any arrangement for surety bonds is a matter strictly between A.A. Will Corporation and Safeco Insurance Company of America. We assume no liability to third parties or to you by issuance of this letter.

Please feel free to contact this office should you have any questions regarding the bonding capacity or technical ability of A.A. Will Corporation.

Sincerely,  
Safeco Insurance Company of America



Sandra C. Lopes, Attorney-in-Fact

THIS POWER OF ATTORNEY IS NOT VALID UNLESS IT IS PRINTED ON RED BACKGROUND.

4259112

This Power of Attorney limits the acts of those named herein, and they have no authority to bind the Company except in the manner and to the extent herein stated.

SAFECO INSURANCE COMPANY OF AMERICA  
SEATTLE, WASHINGTON  
POWER OF ATTORNEY

KNOW ALL PERSONS BY THESE PRESENTS: That Safeco Insurance Company of America (the "Company"), a Washington stock insurance company, pursuant to and by authority of the By-law and Authorization hereinafter set forth, does hereby name, constitute and appoint **MICHAEL J. CUSACK, JEAN M. FEENEY, KEVIN A. WHITE, MARK P. HERENDEEN, SUSAN M. KEDIAN, NATALIE CONEYS, JOHN J. GAMBINO, RICHARD A. LEVERONI, KATHLEEN M. FLANAGAN, NICOLE ROY, JEAN CORREIA, SANDRA C. LOPES, ALL OF THE CITY OF BOSTON, STATE OF MASSACHUSETTS**.....

, each individually if there be more than one named, its true and lawful attorney-in-fact to make, execute, seal, acknowledge and deliver, for and on its behalf as surety and as its act and deed, any and all undertakings, bonds, recognizances and other surety obligations in the penal sum not exceeding **TWO HUNDRED MILLION AND 00/100\*\*\*\*\* DOLLARS (\$ 200,000,000.00\*\*\*\*\* )** each, and the execution of such undertakings, bonds, recognizances and other surety obligations, in pursuance of these presents, shall be as binding upon the Company as if they had been duly signed by the president and attested by the secretary of the Company in their own proper persons.

That this power is made and executed pursuant to and by authority of the following By-law and Authorization:

ARTICLE IV - Execution of Contracts: Section 12. Surety Bonds and Undertakings.

Any officer or other official of the Corporation authorized for that purpose in writing by the Chairman or the President, and subject to such limitations as the Chairman or the President may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Corporation to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact, subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Corporation by their signature and executed, such instruments shall be as binding as if signed by the president and attested by the secretary.

By the following instrument the chairman or the president has authorized the officer or other official named therein to appoint attorneys-in-fact:

Pursuant to Article IV, Section 12 of the By-laws, Garnet W. Elliott, Assistant Secretary of Safeco Insurance Company of America, is authorized to appoint such attorneys-in-fact as may be necessary to act in behalf of the Corporation to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations.

That the By-law and the Authorization set forth above are true copies thereof and are now in full force and effect.

IN WITNESS WHEREOF, this Power of Attorney has been subscribed by an authorized officer or official of the Company and the corporate seal of Safeco Insurance Company of America has been affixed thereto in Plymouth Meeting, Pennsylvania this 24th day of November, 2010.



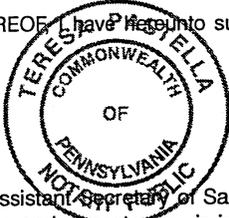
SAFECO INSURANCE COMPANY OF AMERICA

By Garnet W. Elliott  
Garnet W. Elliott, Assistant Secretary

COMMONWEALTH OF PENNSYLVANIA ss  
COUNTY OF MONTGOMERY

On this 24th day of November, 2010, before me, a Notary Public, personally came Garnet W. Elliott, to me known, and acknowledged that he is an Assistant Secretary of Safeco Insurance Company of America; that he knows the seal of said corporation; and that he executed the above Power of Attorney and affixed the corporate seal of Safeco Insurance Company of America thereto with the authority and at the direction of said corporation.

IN TESTIMONY WHEREOF, I have hereunto subscribed my name and affixed my notarial seal at Plymouth Meeting, Pennsylvania, on the day and year first above written.



Notarial Seal  
Teresa Pastella, Notary Public  
Plymouth Twp., Montgomery County  
My Commission Expires Mar. 28, 2013  
Member, Pennsylvania Association of Notaries

By Teresa Pastella  
Teresa Pastella, Notary Public

CERTIFICATE

I, the undersigned, Assistant Secretary of Safeco Insurance Company of America, do hereby certify that the original power of attorney of which the foregoing is a full, true and correct copy, is in full force and effect on the date of this certificate; and I do further certify that the officer or official who executed the said power of attorney is an Assistant Secretary specially authorized by the chairman or the president to appoint attorneys-in-fact as provided in Article IV, Section 12 of the By-laws of Safeco Insurance Company of America.

This certificate and the above power of attorney may be signed by facsimile or mechanically reproduced signatures under and by authority of the following vote of the board of directors of Safeco Insurance Company of America at a meeting duly called and held on the 18th day of September, 2009.

VOTED that the facsimile or mechanically reproduced signature of any assistant secretary of the company, wherever appearing upon a certified copy of any power of attorney issued by the company in connection with surety bonds, shall be valid and binding upon the company with the same force and effect as though manually affixed.

IN TESTIMONY WHEREOF, I have hereunto subscribed my name and affixed the corporate seal of the said company, this 19th day of April, 2011.



By David M. Carey  
David M. Carey, Assistant Secretary

Not valid for mortgage, note, loan, letter of credit, bank deposit, currency rate, interest rate or real value guarantees.

To confirm the validity of this Power of Attorney call 1-610-832-8240 between 9:00 am and 4:30 pm EST on any business day.

Appendix B--Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower tier Covered Transactions

*Instructions for Certification*

1. By signing and submitting this proposal, the prospective lower tier participant is providing the certification set out below.

2. The certification in this clause is a material representation of fact upon which reliance was placed when this transaction was entered into. If it is later determined that the prospective lower tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

3. The prospective lower tier participant shall provide immediate written notice to the person to which this proposal is submitted if at any time the prospective lower tier participant learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

4. The terms "covered transaction", "debarred", "suspended", "ineligible", "lower tier covered transaction", "participant", "person", "primary covered transaction", "principal", "proposal", and "voluntarily excluded", as used in this clause, have the meanings set out in the Definitions and Coverage sections of rules implementing Executive Order 12549. You may contact the person to which this proposal is submitted for assistance in obtaining a copy of those regulations.

5. The prospective lower tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency with which this transaction originated.

6. The prospective lower tier participant further agrees by submitting this proposal that it will include this clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower

Tier Covered Transaction, "without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

7. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that it is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant may decide the method and frequency by which it determines the eligibility of its principals. Each participant may, but is not required to, check the Nonprocurement List.

8. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

9. Except for transactions authorized under paragraph 5 of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

*Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion—Lower Tier Covered Transactions*

(1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

(2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION,  
AND OTHER RESPONSIBILITY MATTERS  
PRIMARY COVERED TRANSACTIONS**

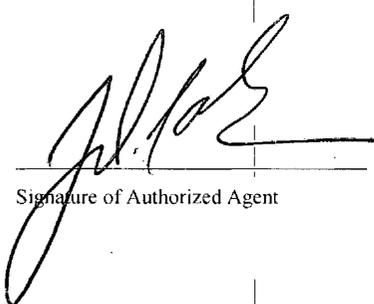
In accordance with the code of Federal Regulations, Part 49 CFR Section 29.5 10, the prospective primary participant

John McNamara, Vice President, being duly sworn (or executed under penalty of perjury under the laws of the United States), certifies to the best of his/her knowledge and belief, that its principals:

- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily Excluded from covered transactions by any Federal department or agency;
- (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (l)(b) of this certification;
- (d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall list exceptions below.

Exceptions will not necessarily result in denial of award, but, will be considered in determining contractor responsibility. For any exception noted, indicate below to whom it applies, the initiating agency, and the dates of the action. Providing false information may result in criminal prosecution or administrative sanctions. If an exception is noted the contractor must contact the Department to discuss the exception prior to award of the contract.

  
\_\_\_\_\_  
Signature of Authorized Agent

4/14/11  
\_\_\_\_\_  
Date

**Certification for Federal-Aid Construction/Consultant Contracts**

**IN ACCORDANCE WITH PUBLIC LAW 101-1210 SECTION 319 (DEPARTMENT OF THE INTERIOR AND RELATED AGENCIES) THE PROSPECTIVE PARTICIPANT CERTIFIES, BY SIGNING AND SUBMITTING THIS BID OR PROPOSAL, TO THE BEST OF HIS/HER KNOWLEDGE AND BELIEF, THAT:**

1. No Federal appropriated funds have been paid or will be paid, by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

2. If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract, grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by Section 1352, Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

**The prospective participant also agrees by submitting his or her bid or proposal that he or she shall require that the language of this certification be included in all lower tier subcontracts, which exceed \$100,000 and that all such subrecipients shall certify and disclose accordingly.**

**(R.I.D.O.T. APPENDIX C)**

## INSTRUCTIONS FOR COMPLETION OF SF-LLL, DISCLOSURE OF LOBBYING ACTIVITIES

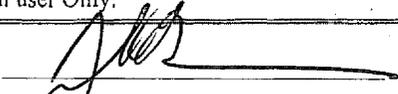
This disclosure form shall be completed by the reporting entity, whether subawardee or prime Federal recipient, at the initiation or receipt of a covered Federal action, or a material change to a previous filing, pursuant to title 31 U.S.C. Section 1352. The filing of a form is required for each payment or agreement to make payment to any lobbying entity for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with a covered Federal action. Use the SF-LLL-A Continuation Sheet for additional information if the space on the form is inadequate. Complete all items that apply for both the initial filing and material change report. Refer to the implementing guidance published by the Office of Management and Budget for additional information.

1. Identify the type of covered Federal action for which lobbying activity is and/or has been secured to influence the outcome of a covered Federal action.
2. Identify the status of the covered Federal action.
3. Identify the appropriate classification of this report. If this is a follow-up report caused by a material change to the information previously reported, enter the year and quarter in which the change occurred. Enter the date of the last previously submitted report by this reporting entity for this covered Federal action.
4. Enter the full name, address, city, state and zip code of the reporting entity. Include Congressional District, if known. Check the appropriate classification of the reporting entity that designates if it is, or expects to be a prime or subaward recipient. Identify the tier of the subawardee (e.g., the first subawardee of the prime is the 1st tier. Subawards include but are not limited to subcontracts, subgrants and contract awards under grants).
5. If the organization filing the report in item 4 checks "Subawardee", then enter the full name, address, city, state and zip code of the prime Federal recipient. Include Congressional District, if known.
6. Enter the name of the Federal agency making the award or loan commitment. Include at least one organizational level below agency name, if known. For example, Department of Transportation, United States Coast Guard.
7. Enter the Federal program name or description for the covered Federal action (item 1). If known, enter the full Catalog of Federal Domestic Assistance (CFDA) number for grants, cooperative agreements, loans, and loan commitments.
8. Enter the most appropriate Federal identifying number available for the Federal action identified in item 1 (e.g., Request for Proposal (RFP) number, Invitation for Bid (IFB) number, grant announcement number, the contract, grant, or loan award number, the application/proposal control number assigned by the Federal agency). Include prefixes, e.g., "RFP-DE-90-001".
9. For a covered Federal action where there has been an award or loan commitment by the Federal agency, enter the Federal amount of the award/loan commitment for the prime entity identified in item 4 or 5.
10. (a) Enter the full name, address, city, state and zip code of the lobbying entity engaged by the reporting entity identified in item 4 to influence the covered Federal action.  
(b) Enter the full names of the individual(s) performing services, and include full address if different from 10(a). Enter Last Name, First Name and Middle Initial (MI).
11. Enter the amount of compensation paid or reasonably expected to be paid by the reporting entity (item 4) to the lobbying entity (item 10). Indicate whether the payment has been made (actual) or will be made (planned). Check all boxes that apply. If this is a material change report, enter the cumulative amount of payment made or planned to be made.
12. Check the appropriate box(es). Check all boxes that apply. If payment is made through an in-kind contribution, specify the nature and value of the in-kind payment.
13. Check the appropriate box(es). Check all boxes that apply. If other, specify nature.
14. Provide a specific and detailed description of the services that the lobbyist has performed, or will be expected to perform, and the date(s) of any services rendered. Include all preparatory and related activity, not just time spent in actual contact with Federal officials. Identify the Federal official(s) or employee(s) contacted or the officer(s), employee(s), or Member(s) of Congress that were contacted.
15. Check whether or not a SF-LLL-A Continuation Sheet(s) is attached.
16. The certifying official shall sign and date the form, print his/her name, title, and telephone number.

Public reporting burden for this collection of information is estimated to average 30 minutes per response, including time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information. Send comments regarding the burden estimate or any other aspect of this collection of information, including suggestions for reducing this burden, to the Office of Management and Budget, Paperwork Reduction Project (0348-0046), Washington, DC 20503.

Complete this form to disclose lobbying activities pursuant to 31 U.S.C 1352 - 0348 - 0046  
(see reverse for public disclosure)

1. Type of Federal Action: Contract	2. Status of Federal Action: bid/offer/application	3. Report Type: initial filing  For Material Change Only: year quarter Date of last report
4. Name and Address of Report Entity: Prime A.A. Will Corporation, 145 Island Street, Stoughton, MA 02072 Congressional District, if known:		5. If Reporting Entity in No. 4 is Subawardee, Name and Address of Prime: N/A  Congressional District, if known:
6. Federal Department Agency: Rhode Island Department of Transportation		7. Federal Program Name/Description: Design/Build Services for the Replacement of Laurel Avenue Br. No. 397 CFDA Number, if applicable:
8. Federal Action Number, if known: N/A		9. Award Amount, if known: N/A
10. a. Name and Address of Lobbying Entity: None		10. b. Individuals Performing Services (including address if different from No. 10a) None.
11. Amount of Payment (check all that apply) None.  Actual Planned		13. Type of Payment: None.  <input type="checkbox"/> Retainer <input type="checkbox"/> One <input type="checkbox"/> Contingent Fee <input type="checkbox"/> Time Fee <input type="checkbox"/> Deferred <input type="checkbox"/> Commission <input type="checkbox"/> Other; Specify;
12. Form of Payment: None.  Cash  In-kind: specify: Nature: Value:		
14. Brief Description of Services Performed or to be Performed and Date(s) of Service; including officer(s), employee (s), or Member(s) contracted, for Payment indicated in Item 11: No lobbying services are being provided.		
15. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. this information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.  Continuation sheet is not attached.		
For Federal user Only:		Authorized for Local Reproduction Standard Form - LLL

Signature: 

Print Name: John McNamara

Title: Vice President

Telephone Number: (781) 341-4800

Date: 4/12/11

## **CONFLICTS DISCLOSURE POLICY**

To ensure that the Rhode Island Department of Transportation (RIDOT) maintains the continued confidence and trust of the people of Rhode Island in carrying out its mission, prospective vendors must disclose any family (or other personal) relationships, associations or connections that the vendor, its affiliates, or employees, may currently have with any RIDOT employee. A Conflicts Disclosure Statement shall be submitted to RIDOT from the following:

- ❖ Owners;
- ❖ Directors;
- ❖ Principals;
- ❖ Officers, board members, or individuals with corporate authority;
- ❖ If the vendor is a partnership, the applicant's partners;
- ❖ If the vendor is a limited liability company, its members and managers;
- ❖ Employees with decision-making authority, including executive directors, managers or individuals in a similar position with corporate authority; and
- ❖ Shareholders with a controlling interest.

**CONFLICTS DISCLOSURE STATEMENT**

**RE:** DESIGN/BUILD Services for the Replacement of the Laurel Avenue Br. No. 397

I, John McNamara hereby certify as follows:

I am employed as a Vice President of A.A. Will Corporation  
 [TITLE] [COMPANY]  
 and to the best of my knowledge:

**PLEASE CHECK THE APPROPRIATE BOX:**

- I have no family or personal relations currently employed either on a full-time or part-time basis at the Rhode Island Department of Transportation.
- I do have family or personal relations currently employed at the Rhode Island Department of Transportation. Please list their name(s), title(s), and RIDOT Division(s) (if known):

NAME	TITLE	RIDOT DIVISION

*If necessary, please add any additional names as attachments hereto.*

**FOR ILLUSTRATIVE PURPOSES, FAMILY RELATIONS SHALL INCLUDE, WHETHER BY BLOOD, ADOPTION OR MARRIAGE, ANY OF THE FOLLOWING RELATIONSHIPS:**

Father, Mother, Son, Daughter, Brother, Sister, Grandfather, Grandmother, Grandson, Granddaughter, Father-In-Law, Mother-In-Law, Brother-In-Law, Sister-In-Law, Son-In-Law, Daughter-In-Law, Stepfather, Stepmother, Stepson, Stepdaughter, Stepbrother, Stepsister, Half-Brother Or Half-Sister, Niece, Nephew, And Cousin

❖ *If you are unsure whether a relationship, association, or connection you have may need to be disclosed, please consult with RIDOT's Legal Office at (401) 222-6510.*

 \_\_\_\_\_  
 SIGNATURE DATE 4-12-11

By signing this form you: (1) certify that the information contained in this form is complete and accurate to the best of your knowledge; and (2) acknowledge your continuing obligation to complete and submit a new Disclosure form when there is any change in your family or personal relations during the course of this Contract.

*This document is used for internal RIDOT purposes only in order to address and avoid any potential conflicts at the inception of the contract process and to avoid any impropriety or the appearance of impropriety during the contract process. Any disclosures made hereto will not prejudice prospective vendors from selection.*

Revised: 4/12/2002

**ANTI-COLLUSION CERTIFICATE FOR CONTRACT AND FORCE ACCOUNT  
[Unsworn Declaration]**

Title 23, United States Code, Section 112(c), requires, as a condition precedent to approval by the Director of Public Roads of the contract for this work, that there be filed an unsworn declaration executed by, on behalf of, the person, firm, association, or corporation submitting the bid certifying that such person, firm, association, or corporation has not either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action, in restraint of free competitive bidding in connection with the submitted bid. This unsworn statement shall be in the form of a declaration executed under penalty of perjury under the laws of the United States.

To the: **STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION, DIVISION OF PUBLIC WORKS**

State of MA

County of Norfolk

I, John McNamara, A.A. Will Corporation, under penalty under the laws of the United States, do depose and say:

On behalf of A.A. Will Corporation, of Stoughton, RI that said Contractor has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with Rhode Island Contract Number , Federal-Aid Project Number , County of , Town-City , Road-Bridge .

## Request for Taxpayer Identification Number and Certification

Give form to the  
 requester. Do not  
 send to the IRS.

**Name (as shown on your income tax return)**  
**A.A. Will Corporation**

**Business name, if different from above**

Check appropriate box:  Individual/Sole proprietor  Corporation  Partnership  
 Limited liability company. Enter the tax classification (D=disregarded entity, C=corporation, P=partnership) ▶ .....  Exempt payee  
 Other (see instructions) ▶

**Address (number, street, and apt. or suite no.)**  
**145 Island Street**

**City, state, and ZIP code**  
**Stoughton, MA 02072**

**Requester's name and address (optional)**

List account number(s) here (optional)

### Part I Taxpayer Identification Number (TIN)

Enter your TIN in the appropriate box. The TIN provided must match the name given on Line 1 to avoid backup withholding. For individuals, this is your social security number (SSN). However, for a resident alien, sole proprietor, or disregarded entity, see the Part I instructions on page 3. For other entities, it is your employer identification number (EIN). If you do not have a number, see *How to get a TIN* on page 3.

Social security number	
or	
Employer identification number	
04	2111361

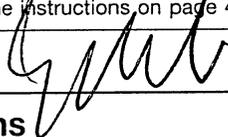
**Note.** If the account is in more than one name, see the chart on page 4 for guidelines on whose number to enter.

### Part II Certification

Under penalties of perjury, I certify that:

- The number shown on this form is my correct taxpayer identification number (or I am waiting for a number to be issued to me), and
- I am not subject to backup withholding because: (a) I am exempt from backup withholding, or (b) I have not been notified by the Internal Revenue Service (IRS) that I am subject to backup withholding as a result of a failure to report all interest or dividends, or (c) the IRS has notified me that I am no longer subject to backup withholding, and
- I am a U.S. citizen or other U.S. person (defined below).

**Certification instructions.** You must cross out item 2 above if you have been notified by the IRS that you are currently subject to backup withholding because you have failed to report all interest and dividends on your tax return. For real estate transactions, item 2 does not apply. For mortgage interest paid, acquisition or abandonment of secured property, cancellation of debt, contributions to an individual retirement arrangement (IRA), and generally, payments other than interest and dividends, you are not required to sign the Certification, but you must provide your correct TIN. See the instructions on page 4.

**Sign Here**      Signature of U.S. person ▶       Date ▶ 4/8/11

### General Instructions

Section references are to the Internal Revenue Code unless otherwise noted.

#### Purpose of Form

A person who is required to file an information return with the IRS must obtain your correct taxpayer identification number (TIN) to report, for example, income paid to you, real estate transactions, mortgage interest you paid, acquisition or abandonment of secured property, cancellation of debt, or contributions you made to an IRA.

Use Form W-9 only if you are a U.S. person (including a resident alien), to provide your correct TIN to the person requesting it (the requester) and, when applicable, to:

- Certify that the TIN you are giving is correct (or you are waiting for a number to be issued),
- Certify that you are not subject to backup withholding, or
- Claim exemption from backup withholding if you are a U.S. exempt payee. If applicable, you are also certifying that as a U.S. person, your allocable share of any partnership income from a U.S. trade or business is not subject to the withholding tax on foreign partners' share of effectively connected income.

**Note.** If a requester gives you a form other than Form W-9 to request your TIN, you must use the requester's form if it is substantially similar to this Form W-9.

**Definition of a U.S. person.** For federal tax purposes, you are considered a U.S. person if you are:

- An individual who is a U.S. citizen or U.S. resident alien,
- A partnership, corporation, company, or association created or organized in the United States or under the laws of the United States,
- An estate (other than a foreign estate), or
- A domestic trust (as defined in Regulations section 301.7701-7).

**Special rules for partnerships.** Partnerships that conduct a trade or business in the United States are generally required to pay a withholding tax on any foreign partners' share of income from such business. Further, in certain cases where a Form W-9 has not been received, a partnership is required to presume that a partner is a foreign person, and pay the withholding tax. Therefore, if you are a U.S. person that is a partner in a partnership conducting a trade or business in the United States, provide Form W-9 to the partnership to establish your U.S. status and avoid withholding on your share of partnership income.

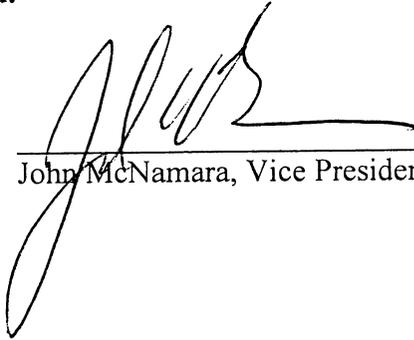
The person who gives Form W-9 to the partnership for purposes of establishing its U.S. status and avoiding withholding on its allocable share of net income from the partnership conducting a trade or business in the United States is in the following cases:

- The U.S. owner of a disregarded entity and not the entity,

3.0 ADMINISTRATIVE REQUIREMENTS GENERAL INSTRUCTIONS AND NOTIFICATIONS

**CERTIFICATE OF AUTHORITY:**

A.A. Will Corporation does **NOT** have a current Certificate of Authority and is acknowledging non-compliance with this requirement. If selected for the project, A.A. Will Corporation will expedite acquisition to obtain a Certificate of Authority **prior to award.**



---

John McNamara, Vice President 4/20/11

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION,  
AND OTHER RESPONSIBILITY MATTERS  
PRIMARY COVERED TRANSACTIONS**

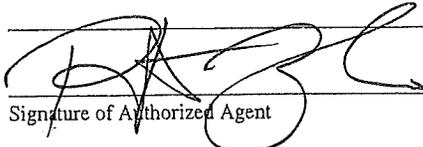
In accordance with the code of Federal Regulations, Part 49 CFR Section 29. 510, the prospective primary participant Richard Bernardo, PE (name of Authorized Agent), Senior Vice President (Title), being duly sworn (or under penalty of perjury under the laws of the United States), certifies to the best of his/her knowledge and belief, that its principals:

- a. Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily excluded from covered transactions by any Federal department or agency;
- b. Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- c. Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (1) (b) of this certification;
- d. Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Where the prospective primary participant is unable to certify any of the statements in this certification, such prospective participant shall list exceptions below.

Exceptions will not necessarily result in denial of award, but, will be considered in determining contractor responsibility. For any exception noted, indicate below to whom it applies, the initiating agency, and the dates of the action. Providing false information may result in criminal prosecution or administrative sanctions. If an exception is noted the contractor must contact the Department to discuss the exception prior to award of the contract.

\_\_\_\_\_  
\_\_\_\_\_

  
\_\_\_\_\_  
Signature of Authorized Agent

March 21, 2011

\_\_\_\_\_  
Date

# DISCLOSURE OF LOBBYING ACTIVITIES

Complete this form to disclose lobbying activities pursuant to 31 U.S.C. 1352  
(See reverse for public burden disclosure.)

<p>1. Type of Federal Action:</p> <p><input checked="" type="checkbox"/> a. contract  <input type="checkbox"/> b. grant  <input type="checkbox"/> c. cooperative agreement  <input type="checkbox"/> d. loan  <input type="checkbox"/> e. loan guarantee  <input type="checkbox"/> f. loan insurance</p>	<p>2. Status of Federal Action:</p> <p><input checked="" type="checkbox"/> a. bid/offer/application  <input type="checkbox"/> b. initial award  <input type="checkbox"/> c. post-award</p>	<p>3. Report Type:</p> <p><input checked="" type="checkbox"/> a. initial filing  <input type="checkbox"/> b. material change</p> <p>For Material Change Only:  year _____ quarter _____</p> <p>date of last report _____</p>
<p>4. Name and Address of Reporting Entity:</p> <p><input checked="" type="checkbox"/> Prime      <input type="checkbox"/> Subawardee  Tier _____, if known:</p> <p>Maguire Group Inc  225 Chapman Street, 4<sup>th</sup> Fl, Providence, RI 02905  Congressional District, if known: _____</p>	<p>5. If Reporting Entity in No. 4 is Subawardee, Enter Name and Address of Prime:</p> <p>N/A</p> <p>Congressional District, if known: _____</p>	
<p>6. Federal Department/Agency:</p> <p>Rhode Island Department of Transportation</p>	<p>7. Federal Program Name/Description</p> <p>DESIGN/BUILD Services for the Replacement of the Laurel Avenue Br. No. 397  CFDA Number, if applicable: _____</p>	
<p>8. Federal Action Number, if known: N/A</p>	<p>9. Award Amount, if known: N/A  \$ _____</p>	
<p>10. a. Name and Address of Lobbying Entity (if individual, last name, first name, MI):</p> <p>None  (attach Continuation Sheet(s))</p>	<p>b. Individuals Performing Services (including address if different from No. 10a) (last name, first name, MI): None</p> <p>SF-LLL-A, if necessary) _____</p>	
<p>11. Amount of Payment (check all that apply): None  \$ _____      <input type="checkbox"/> actual      <input type="checkbox"/> planned</p>	<p>13. Type of Payment (check all that apply): None</p> <p><input type="checkbox"/> a. retainer  <input type="checkbox"/> b. one-time fee  <input type="checkbox"/> c. commission  <input type="checkbox"/> d. contingent fee  <input type="checkbox"/> e. deferred  <input type="checkbox"/> f. other; specify: _____</p>	
<p>12. Form of Payment (check all that apply): None</p> <p><input type="checkbox"/> a. cash  <input type="checkbox"/> b. in-kind; specify: nature _____ value _____</p>		
<p>14. Brief Description of Services Performed or to be Performed and Date(s) of Service, including officer(s), employee(s), or Member(s) contacted, for Payment Indicated in Item 11:</p> <p>No lobbying services are being provided</p> <p>(attach Continuation Sheet(s) SF-LLL-A if necessary)</p>		
<p>15. Continuation Sheet(s) SF-LLL-A attached:    <input type="checkbox"/> Yes    <input checked="" type="checkbox"/> No</p>		
<p>16. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. This information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.</p>	<p>Signature: </p> <p>Print Name: Richard Bernardo, PE</p> <p>Title: Senior Vice President</p> <p>Telephone No.: 401.272.6000      Date: 3/21/11</p>	
<p>Federal Use Only:</p>		<p>Authorized for Local Reproduction  Standard Form - LLL-A</p>



Revised: 4/12/2002

ANTI-COLLUSION CERTIFICATE FOR CONTRACT AND FORCE ACCOUNT  
[Unsworn Declaration]

Title 23, United States Code, Section 112(c), requires, as a condition precedent to approval by the Director of Public Roads of the contract for this work, that there be filed an unsworn declaration executed by, on behalf of, the person, firm, association, or corporation submitting the bid certifying that such person, firm, association, or corporation has not either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action, in restraint of free competitive bidding in connection with the submitted bid. This unsworn statement shall be in the form of a declaration executed under penalty of perjury under the laws of the United States.

To the: STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION, DIVISION OF PUBLIC WORKS

State of RI

County of Providence

I, Richard A. Bernards Mogure Group, under penalty under the laws of the United States, do depose and say:

On behalf of Mogure Group, of Providence RI that said Contractor has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with Rhode Island Contract Number , Federal-Aid Project Number , County of , Town-City , Road-Bridge .

State of Rhode Island
PAYER'S REQUEST FOR TAXPAYER
IDENTIFICATION NUMBER AND CERTIFICATION

THE IRS REQUIRES THAT YOU FURNISH YOUR TAXPAYER IDENTIFICATION NUMBER TO US. FAILURE TO PROVIDE THIS INFORMATION CAN RESULT IN A \$50 PENALTY BY THE IRS. IF YOU ARE AN INDIVIDUAL, PLEASE PROVIDE US WITH YOUR SOCIAL SECURITY NUMBER (SSN) IN THE SPACE INDICATED BELOW. IF YOU ARE A COMPANY OR A CORPORATION, PLEASE PROVIDE US WITH YOUR EMPLOYER IDENTIFICATION NUMBER (EIN) WHERE INDICATED.

Taxpayer Identification Number (T.I.N.)

Enter your taxpayer identification number in the appropriate box. For most individuals, this is your social security number.

Social Security No. (SSN)

Empty boxes for Social Security No. (SSN)

Employer ID No. (EIN)

05 0318211

NAME Richard Bernardo, PE

ADDRESS 225 Chapman Street, 4th Floor

(REMITTANCE ADDRESS, IF DIFFERENT)

CITY, STATE AND ZIP CODE Providence, RI 02905

CERTIFICATION: Under penalties of perjury, I certify that:

- (1) The number shown on this form is my correct Taxpayer Identification Number (or I am waiting for a number to be issued to me), and
(2) I am not subject to backup withholding because either: (A) have not been notified by the Internal Revenue Service (IRS) that I am subject to backup withholding as a result of a failure to report all interest or dividends, or (B) the IRS has notified me that I am no longer subject to backup withholding.

Certification Instructions -- You must cross out item (2) above if you have been notified by the IRS that you are subject to backup withholding because of under-reporting interest or dividends on your tax return. However, if after being notified by IRS that you were subject to backup withholding you received another notification from IRS that you are no longer subject to backup withholding, do not cross out item (2).

PLEASE SIGN HERE

SIGNATURE [Handwritten Signature]

TITLE Senior VP

DATE 3/21/11

TEL NO. 401.272.6000

BUSINESS DESIGNATION:

- Please Check One: Individual [ ] Medical Services Corporation [ ] Government/Nonprofit Corporation [ ]
Partnership [ ] Corporation [X] Trust/Estate [ ] Legal Services Corporation [ ]

NAME: Be sure to enter your full and correct name as listed in the IRS file for you or your business.

ADDRESS, CITY, STATE AND ZIP CODE: Enter your primary business address and remittance address if different from your primary address). If you operate a business at more than one location, adhere to the following:

- 1) Same T.I.N. with more than one location -- attach a list of location addresses with remittance address for each location and indicate to which location the year-end tax information return should be mailed.
2) Different T.I.N. for each different location -- submit a completed W-9 form for each T.I.N. and location. (One year-end tax information return will be reported for each T.I.N. and remittance address.)

CERTIFICATION -- Sign the certification, enter your title, date, and your telephone number (including area code and extension).

BUSINESS TYPE CHECK-OFF -- Check the appropriate box for the type of business ownership.

Mail to: Supplier Coordinator, One Capitol Hill, Providence, RI 02908

**SUBCONTRACTOR CERTIFICATION – OPTION 2 - ENTITY**

(To be used by an entity (partnership, corporation, limited liability partnership, etc.) doing – or intending to do business with the State of Rhode Island)

**THIS SECTION TO BE COMPLETED BY THE BIDDER/VENDOR**

Vendor Name and ID: «VENDOR\_NAME»

Purchase Order # \_\_\_\_\_

RFP # \_\_\_\_\_

Bid # \_\_\_\_\_

**THIS SECTION TO BE COMPLETED BY THE SUBCONTRACTOR**

Subcontractor Name Maquire Group Inc.

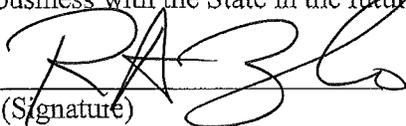
Address 225 Chapman Street

City, State Zip Providence, RI

Telephone Number 401-272-6000

I, Richard Bernardo the Sr. Vice President of Maquire Group Inc.  
(Full name) (Title) (Name of entity –“Entity”)

hereby certify that I am a representative of said Entity and am duly authorized to execute this Vendor Certification on behalf of the Entity; that said Entity registered to utilize the E-Verify program on August 5, 2008 and that the Entity utilizes the services of the E-Verify program to ensure compliance with federal and state requirements. On behalf of the Entity, I understand and agree that the Entity is required to continue to utilize the services of the E-Verify program for as long as the Entity continues to do business with the State of Rhode Island and failure to continue to utilize the services of the E-Verify program will adversely affect the Entity’s ability to continue to do business with the State of Rhode Island and will affect the Entity’s ability to do business with the State in the future.



Date: 4/4/11

(Signature)

Richard A. Bernardo

(Print name)

**NOTARY PUBLIC**

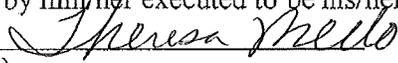
STATE OF Rhode Island

COUNTY OF Providence

In Providence in said County on the 4th day of

April, 2008, before me personally appeared

Richard A. Bernardo, to me known and known by me to be the party executing the above Vendor Certification on behalf of the Entity, and he/she acknowledged said document, by him/her executed to be his/her free act and deed of said Entity.



(Signature)

(Printed name) Theresa Mello My commission expires on: 6/28/2013

Complete and Return To: State of Rhode Island, Division of Purchases, P O. Box 41544, Providence, Rhode Island 02940



STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
Department of Business Regulation  
DIVISION OF DESIGN PROFESSIONALS  
1511 Pontiac Avenue, Bldg. 68-2  
Cranston, RI 02920  
(401) 462-9530 Fax: (401) 462-9532 www.bdp.state.ri.us

30 September 2010

MAGUIRE GROUP, INC.

33 COMMERCIAL STREET, SUITE 1  
FOXBOROUGH, MA 02035

To The Principal in Charge:

Sole No.  
Part No.  
Corp No. 5117  
LLC No.  
LLP No.

Your renewal application for a Certificate of Authorization (COA) has been approved in the CIVIL, STRUCTURAL & ELECTRICAL discipline.

Your COA to practice engineering as defined in Chapter 5-8-24 of the General Laws of Rhode Island is renewed **through 30 June 2012.**

Attached please find a COA validation sticker. This sticker is to be placed in the upper right hand corner of your COA Certificate which was previously provided to you, at the time of the initial issuance.

Very truly yours,

**BOARD OF REGISTRATION FOR  
PROFESSIONAL ENGINEERS**

Christopher Duhamel, PE, PLS  
Secretary

Attachment



Appendix B--Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower tier Covered Transactions

*Instructions for Certification*

1. By signing and submitting this proposal, the prospective lower tier participant is providing the certification set out below.

2. The certification in this clause is a material representation of fact upon which reliance was placed when this transaction was entered into. If it is later determined that the prospective lower tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

3. The prospective lower tier participant shall provide immediate written notice to the person to which this proposal is submitted if at any time the prospective lower tier participant learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

4. The terms "covered transaction", "debarred", "suspended", "ineligible", "lower tier covered transaction", "participant", "person", "primary covered transaction", "principal", "proposal", and "voluntarily excluded", as used in this clause, have the meanings set out in the Definitions and Coverage sections of rules implementing Executive Order 12549. You may contact the person to which this proposal is submitted for assistance in obtaining a copy of those regulations.

5. The prospective lower tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency with which this transaction originated.

6. The prospective lower tier participant further agrees by submitting this proposal that it will include this clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower

Tier Covered Transaction, "without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

7. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that it is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant may decide the method and frequency by which it determines the eligibility of its principals. Each participant may, but is not required to, check the Nonprocurement List.

8. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

9. Except for transactions authorized under paragraph 5 of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

*Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion—Lower Tier Covered Transactions*

(1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

(2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION,  
AND OTHER RESPONSIBILITY MATTERS  
PRIMARY COVERED TRANSACTIONS**

In accordance with the code of Federal Regulations, Part 49 CFR Section 29.5 10, the prospective primary participant

LAMSON ENGINEERING CORP., KING C. LAM, being duly sworn (or executed under penalty of perjury under the laws of the United States), certifies to the best of his/her knowledge and belief, that its principals:

- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily Excluded from covered transactions by any Federal department or agency;
- (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification;
- (d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall list exceptions below.

Exceptions will not necessarily result in denial of award, but, will be considered in determining contractor responsibility. For any exception noted, indicate below to whom it applies, the initiating agency, and the dates of the action. Providing false information may result in criminal prosecution or administrative sanctions. If an exception is noted the contractor must contact the Department to discuss the exception prior to award of the contract.

## INSTRUCTIONS FOR COMPLETION OF SF-LLL, DISCLOSURE OF LOBBYING ACTIVITIES

This disclosure form shall be completed by the reporting entity, whether subawardee or prime Federal recipient, at the initiation or receipt of a covered Federal action, or a material change to a previous filing, pursuant to title 31 U.S.C. Section 1352. The filing of a form is required for each payment or agreement to make payment to any lobbying entity for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with a covered Federal action. Use the SF-LLL-A Continuation Sheet for additional information if the space on the form is inadequate. Complete all items that apply for both the initial filing and material change report. Refer to the implementing guidance published by the Office of Management and Budget for additional information.

1. Identify the type of covered Federal action for which lobbying activity is and/or has been secured to influence the outcome of a covered Federal action.
2. Identify the status of the covered Federal action.
3. Identify the appropriate classification of this report. If this is a follow-up report caused by a material change to the information previously reported, enter the year and quarter in which the change occurred. Enter the date of the last previously submitted report by this reporting entity for this covered Federal action.
4. Enter the full name, address, city, state and zip code of the reporting entity. Include Congressional District, if known. Check the appropriate classification of the reporting entity that designates if it is, or expects to be a prime or subaward recipient. Identify the tier of the subawardee, e.g., the first subawardee of the prime is the 1st tier. Subawards include but are not limited to subcontracts, subgrants and contract awards under grants.
5. If the organization filing the report in item 4 checks "Subawardee", then enter the full name, address, city, state and zip code of the prime Federal recipient. Include Congressional District, if known.
6. Enter the name of the Federal agency making the award or loan commitment. Include at least one organizational level below agency name, if known. For example, Department of Transportation, United States Coast Guard.
7. Enter the Federal program name or description for the covered Federal action (item 1). If known, enter the full Catalog of Federal Domestic Assistance (CFDA) number for grants, cooperative agreements, loans, and loan commitments.
8. Enter the most appropriate Federal identifying number available for the Federal action identified in item 1 (e.g., Request for Proposal (RFP) number, Invitation for Bid (IFB) number, grant announcement number, the contract, grant, or loan award number, the application/proposal control number assigned by the Federal agency). Include prefixes, e.g., "RFP-DE-90-001".
9. For a covered Federal action where there has been an award or loan commitment by the Federal agency, enter the Federal amount of the award/loan commitment for the prime entity identified in item 4 or 5.
10. (a) Enter the full name, address, city, state and zip code of the lobbying entity engaged by the reporting entity identified in item 4 to influence the covered Federal action.  
(b) Enter the full names of the individual(s) performing services, and include full address if different from 10(a). Enter Last Name, First Name and Middle Initial (MI).
11. Enter the amount of compensation paid or reasonably expected to be paid by the reporting entity (item 4) to the lobbying entity (item 10). Indicate whether the payment has been made (actual) or will be made (planned). Check all boxes that apply. If this is a material change report, enter the cumulative amount of payment made or planned to be made.
12. Check the appropriate box(es). Check all boxes that apply. If payment is made through an in-kind contribution, specify the nature and value of the in-kind payment.
13. Check the appropriate box(es). Check all boxes that apply. If other, specify nature.
14. Provide a specific and detailed description of the services that the lobbyist has performed, or will be expected to perform, and the date(s) of any services rendered. Include all preparatory and related activity, not just time spent in actual contact with Federal officials. Identify the Federal official(s) or employee(s) contacted or the officer(s), employee(s), or Member(s) of Congress that were contacted.
15. Check whether or not a SF-LLL-A Continuation Sheet(s) is attached.
16. The certifying official shall sign and date the form, print his/her name, title, and telephone number.

Public reporting burden for this collection of information is estimated to average 30 minutes per response, including time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information. Send comments regarding the burden estimate or any other aspect of this collection of information, including suggestions for reducing this burden, to the Office of Management and Budget, Paperwork Reduction Project (0348-0046), Washington, DC 20503.

**Certification for Federal-Aid Construction/Consultant Contracts**

**IN ACCORDANCE WITH PUBLIC LAW 101-1210 SECTION 319 (DEPARTMENT OF THE INTERIOR AND RELATED AGENCIES) THE PROSPECTIVE PARTICIPANT CERTIFIES, BY SIGNING AND SUBMITTING THIS BID OR PROPOSAL, TO THE BEST OF HIS/HER KNOWLEDGE AND BELIEF, THAT:**

1. No Federal appropriated funds have been paid or will be paid, by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

2. If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract, grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by Section 1352, Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

**The prospective participant also agrees by submitting his or her bid or proposal that he or she shall require that the language of this certification be included in all lower tier subcontracts, which exceed \$100,000 and that all such subrecipients shall certify and disclose accordingly.**

**(R.I.D.O.T. APPENDIX C)**

Complete this form to disclose lobbying activities pursuant to 31 U.S.C 1352 - 0348 - 0046  
 (see reverse for public disclosure)

**NOT APPLICABLE / NONE**

1. Type of Federal Action:	2. Status of Federal Action:	3. Report Type:  For Material Change Only: year quarter Date of last report
4. Name and Address of Report Entity:  Congressional District, if known:		5. If Reporting Entity in No. 4 is Subawardee, Name and Address of Prime:  Congressional District, if known:
6. Federal Department Agency:	7. Federal Program Name/Description:  CFDA Number, if applicable:	
8. Federal Action Number, if known:	9. Award Amount, if known:	
10. a. Name and Address of Lobbying Entity:	10. b. Individuals Performing Services (including address if different from No. 10a)	
11. Amount of Payment (check all that apply)  Actual Planned	13. Type of Payment:  _ Retainer    _ One    _ Contingent Fee _ Time Fee        _ Deferred _ Commission    _ Other; Specify;	
12. Form of Payment:  Cash  In-kind: specify: Nature: Value:		
14. Brief Description of Services Performed or to be Performed and Date(s) of Service; including officer(s), employee (s), or Member(s) contracted, for Payment indicated in Item 11:		
15. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. This information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.		
For Federal user Only:	Authorized for Local Reproduction Standard Form - LLL	

**DISCLOSURE OF LOBBYING ACTIVITIES**  
CONTINUATION SHEET *NOT APPLICABLE / NONE*

Reporting Entity: \_\_\_\_\_ Page \_\_\_\_\_ of \_\_\_\_\_

## **CONFLICTS DISCLOSURE POLICY**

To ensure that the Rhode Island Department of Transportation (RIDOT) maintains the continued confidence and trust of the people of Rhode Island in carrying out its mission, prospective vendors must disclose any family (or other personal) relationships, associations or connections that the vendor, its affiliates, or employees, may currently have with any RIDOT employee. A Conflicts Disclosure Statement shall be submitted to RIDOT from the following:

- ❖ Owners;
- ❖ Directors;
- ❖ Principals;
- ❖ Officers, board members, or individuals with corporate authority;
- ❖ If the vendor is a partnership, the applicant's partners;
- ❖ If the vendor is a limited liability company, its members and managers;
- ❖ Employees with decision-making authority, including executive directors, managers or individuals in a similar position with corporate authority; and
- ❖ Shareholders with a controlling interest.

**CONFLICTS DISCLOSURE STATEMENT**RE: Design/Build Replacement of Laurel Ave. Bridge # 397I, KIN C. LAM hereby certify as follows:I am employed as a President of LAMSON ENGINEERING CORP.  
[TITLE] [COMPANY]

and to the best of my knowledge:

**PLEASE CHECK THE APPROPRIATE BOX:**

- I have no family or personal relations currently employed either on a full-time or part-time basis at the Rhode Island Department of Transportation.
- I do have family or personal relations currently employed at the Rhode Island Department of Transportation. Please list their name(s), title(s), and RIDOT Division(s) (if known):

NAME	TITLE	RIDOT DIVISION

*If necessary, please add any additional names as attachments hereto.***FOR ILLUSTRATIVE PURPOSES, FAMILY RELATIONS SHALL INCLUDE, WHETHER BY BLOOD, ADOPTION OR MARRIAGE, ANY OF THE FOLLOWING RELATIONSHIPS:**

Father, Mother, Son, Daughter, Brother, Sister, Grandfather, Grandmother, Grandson, Granddaughter, Father-In-Law, Mother-In-Law, Brother-In-Law, Sister-In-Law, Son-In-Law, Daughter-In-Law, Stepfather, Stepmother, Stepson, Stepdaughter, Stepbrother, Stepsister, Half-Brother Or Half-Sister, Niece, Nephew, And Cousin

- ❖ If you are unsure whether a relationship, association, or connection you have may need to be disclosed, please consult with RIDOT's Legal Office at (401) 222-6510.

Kin C Lam 4/19/2011  
SIGNATURE DATE

By signing this form you: (1) certify that the information contained in this form is complete and accurate to the best of your knowledge; and (2) acknowledge your continuing obligation to complete and submit a new Disclosure form when there is any change in your family or personal relations during the course of this Contract.

*This document is used for internal RIDOT purposes only in order to address and avoid any potential conflicts at the inception of the contract process and to avoid any impropriety or the appearance of impropriety during the contract process. Any disclosures made hereto will not prejudice prospective vendors from selection.*

Revised: 4/12/2002

**ANTI-COLLUSION CERTIFICATE FOR CONTRACT AND FORCE ACCOUNT  
[Unsworn Declaration]**

Title 23, United States Code, Section 112(c), requires, as a condition precedent to approval by the Director of Public Roads of the contract for this work, that there be filed an unsworn declaration executed by, on behalf of, the person, firm, association, or corporation submitting the bid certifying that such person, firm, association, or corporation has not either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action, in restraint of free competitive bidding in connection with the submitted bid. This unsworn statement shall be in the form of a declaration executed under penalty of perjury under the laws of the United States.

To the: **STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION, DIVISION OF PUBLIC WORKS**

State of MASSACHUSETTS

County of MIDDLESEX

I, KIN C. LAM, \_\_\_\_\_, under penalty under the laws of the United States, do depose and say:

On behalf of LAMSON ENGINEERING, of \_\_\_\_\_ that said Contractor has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with Rhode Island Contract Number , Federal-Aid Project Number , County of , Town-City , Road-Bridge .

RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR

CONTRACT/PROPOSAL NAME:

Design/Build Replacement of  
Laurel Ave. Bridge # 397

NAME OF BIDDER/RESPONDENT:

A. A. WILL CORP.  
Maguire Group Inc.

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor LAMSON ENGINEERING CORPORATION  
Address 437 CHERRY STREET #109  
City NEWTON State MA Zip 02465

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

The above named bidder and subcontractor mutually accepts the commitment total estimated for the Unit Prices and Quantities. This commitment total is based on estimated quantities only and most likely will vary up or down as the project is completed. Final compensation and DBE credit will be based on actual quantities of work performed and accepted during the pursuance of work. The listed amount represents the entire dollar amount quoted based on these estimated quantities. No conversations, verbal agreements, and/or other forms of non-written representations shall serve to add, delete, or modify the terms as stated. This document shall not serve in any manner as an actual subcontract between the two parties. A separate subcontractor agreement will describe in detail the contractual obligations of the bidder and the DBE subcontractor.

**Affirmation**

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

LAMSON ENGINEERING CORP.

Name of DBE Subcontractor

\_\_\_\_\_  
Name of Bidder

John G. Lamson / President

Signature / Title

4/19/2011 Date

\_\_\_\_\_  
Signature / Title

\_\_\_\_\_  
Date

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS



Department of Administration  
**Minority Business Enterprise Compliance Office**

One Capitol Hill  
Providence, RI 02908-5860  
Office: (401) 574-8670  
Fax: (401) 574-8387

RI Relay: 711  
[www.mbe.ri.gov](http://www.mbe.ri.gov)

August 13, 2010

Mr. Kin Lam  
Lamson Engineering Corporation  
437 Cherry Street, #109  
Newton, MA 02465

Dear Mr. Lam:

Based on the recertification application and supplemental information provided by you, your application for recertification for the State of Rhode Island Minority Business Enterprise Program has been approved. Your company has been approved as a **MBE/DBE** to conduct business primarily as a **“consulting engineering services firm specializing in structural, geotechnical, and civil engineering”** firm under SIC Code 8711 and under NAICS Code 541330. Your “Minority Business Certification Number” which you can use as proof of your status is MBCN 245. If your company has been identified as a DBE, be advised that the MBE Compliance Office, acting as certification agent for RIDOT, RIAC, and RIPTA, has determined that your firm meets the certification criteria as established by U.S. DOT under 49 CFR Part 26.

Your certification is valid until **8/31/2013** unless revoked sooner based on a determination of ineligibility. It is your responsibility to notify the Minority Business Enterprise Compliance Office of any changes in the ownership or control of your business within 30 days of such changes. At the end of your certification period, if you wish to recertify, your company will undergo a substantive review, including a new site visit, as applicable.

In order to maintain your certification during the certification period, you must submit your annual review package sixty (60) days prior to your annual review date which is **8/31/2011**. Your annual review package must include: (a) a completed No Change Affidavit; (b) current corporate financial statements; (c) current corporate and personal tax returns including all schedules and attachments; (d) completed Personal Financial Statement and Statement of Disadvantage Forms; and (e) a copy of your current certification letter from your home state UCP if firm is not based in Rhode Island. Failure to submit your annual review package will result in an administrative removal of your certification.

We wish you success in the State of Rhode Island’s Minority Business Enterprise Program and the Department of Transportation’s Disadvantaged Business Enterprise Program; and, if we can be of further assistance to you, please contact this office.

Sincerely,

Charles C. Newton, Administrator  
MBE Compliance Office

An Equal Opportunity Affirmative Action Employer

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION,  
AND OTHER RESPONSIBILITY MATTERS  
PRIMARY COVERED TRANSACTIONS**

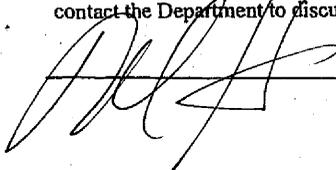
In accordance with the code of Federal Regulations, Part 49 CFR Section 29.5 10, the prospective primary participant

NARRACANSETT ENGINEERING INC., being duly sworn (or executed under penalty of perjury under the laws of the United States), certifies to the best of his/her knowledge and belief, that its principals:

- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily Excluded from covered transactions by any Federal department or agency;
- (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification;
- (d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall list exceptions below.

Exceptions will not necessarily result in denial of award, but, will be considered in determining contractor responsibility. For any exception noted, indicate below to whom it applies, the initiating agency, and the dates of the action. Providing false information may result in criminal prosecution or administrative sanctions. If an exception is noted the contractor must contact the Department to discuss the exception prior to award of the contract.

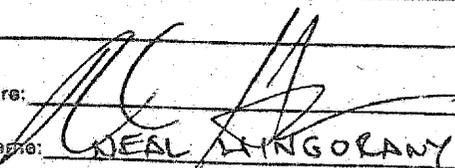
 V.P. 04.05.11

Complete this form to disclose lobbying activities pursuant to 31 U.S.C 1352 - 0348 - 0046  
(see reverse for public disclosure)

1. Type of Federal Action: <b>CONTRACT</b>	2. Status of Federal Action: <b>BID</b>	3. Report Type: For Material Change Only: year quarter Date of last report
4. Name and Address of Report Entity: <b>NARRAGANSETT ENGINEERING INC 302 E. MAIN RD PORTSMOUTH Congressional District, if known: RI 02811</b>		5. If Reporting Entity in No. 4 is Subawardee, Name and Address of Prime: <b>A. A. WIL CORP. 145 ISLAND ST. STOUGHTON MA 02072</b> Congressional District, if known:
6. Federal Department Agency:	7. Federal Program Name/Description: CFDA Number, if applicable:	
8. Federal Action Number, if known:	9. Award Amount, if known:	
10. a. Name and Address of Lobbying Entity:	10. b. Individuals Performing Services (including address if different from No. 10a): <b>NARRAGANSETT ENGINEERING INC. 302 E. MAIN RD PORTSMOUTH RI 02811</b>	
11. Amount of Payment (check all that apply)  Actual Planned	13. Type of Payment:  <input type="checkbox"/> Retainer <input checked="" type="checkbox"/> One <input type="checkbox"/> Contingent Fee <input type="checkbox"/> Time Fee <input type="checkbox"/> Deferred <input type="checkbox"/> Commission <input type="checkbox"/> Other; Specify;	
12. Form of Payment:  Cash <input checked="" type="checkbox"/>  In-kind: specify: Nature: Value:		
14. Brief Description of Services Performed or to be Performed and Date(s) of Service; including officer(s), employee (s), or Member(s) contracted, for Payment indicated in Item 11: <b>RIPOT BID T448315 DESIGN/BLIND - LAUREL AVE BRIDGE</b>		
15. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. This information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.		
For Federal user Only:	Authorized for Local Reproduction Standard Form - LLL	

# DISCLOSURE OF LOBBYING ACTIVITIES

Complete this form to disclose lobbying activities pursuant to 31 U.S.C. 1352.  
(See reverse for public burden disclosure)

<p>1. Type of Federal Action:</p> <p><input checked="" type="checkbox"/> a. contract  <input type="checkbox"/> b. grant  <input type="checkbox"/> c. cooperative agreement  <input type="checkbox"/> d. loan  <input type="checkbox"/> e. loan guarantee  <input type="checkbox"/> f. loan insurance</p>	<p>2. Status of Federal Action:</p> <p><input checked="" type="checkbox"/> a. bid/offer/application  <input type="checkbox"/> b. initial award  <input type="checkbox"/> c. post-award</p>	<p>3. Report Type:</p> <p><input checked="" type="checkbox"/> a. initial filing  <input type="checkbox"/> b. material change</p> <p>For Material Change Only:  year _____ quarter _____</p> <p>date of last report _____</p>
<p>4. Name and Address of Reporting Entity:</p> <p><input type="checkbox"/> Prime <input checked="" type="checkbox"/> Subawardee  Tier _____, if known:</p> <p><b>NARRAGANSETT ENGINEERING INC</b>  <b>3102 E. MAIN RD FORTSMOUTH RI 02871</b>  Congressional District, if known:</p>		<p>5. If Reporting Entity in No. 4 is Subawardee, Enter Name and Address of Prime:</p> <p><b>MCGUIRE GROUP</b>  <b>225 CHAPMAN ST. 4TH FLOOR</b>  <b>PROVIDENCE RI 02905</b>  Congressional District, if known:</p>
<p>6. Federal Department/Agency:</p> <p>_____</p>	<p>7. Federal Program Name/Description</p> <p>_____</p> <p>CFDA Number, if applicable: _____</p>	
<p>8. Federal Action Number, if known:</p> <p>_____</p>	<p>9. Award Amount, if known:</p> <p>\$ _____</p>	
<p>10. a. Name and Address of Lobbying Entity (if individual, last name, first name, MI):</p> <p><b>N/A</b></p> <p>(attach Continuation Sheet(s))</p>	<p>b. Individuals Performing Services (including address if different from No. 10a) (last name, first name, MI):</p> <p>_____</p> <p>SF-LLL-A, if necessary)</p>	
<p>11. Amount of Payment (check all that apply):</p> <p>\$ <u>      </u> <input type="checkbox"/> actual <input type="checkbox"/> planned</p> <p><b>TED</b></p>	<p>13. Type of Payment (check all that apply):</p> <p><b>TED</b></p> <p><input type="checkbox"/> a. retainer  <input type="checkbox"/> b. one-time fee  <input type="checkbox"/> c. commission  <input type="checkbox"/> d. contingent fee  <input type="checkbox"/> e. deferred  <input type="checkbox"/> f. other; specify: _____</p>	
<p>12. Form of Payment (check all that apply):</p> <p><input checked="" type="checkbox"/> a. cash  <input type="checkbox"/> b. in-kind; specify: nature _____ value _____</p>		
<p>14. Brief Description of Services Performed or to be Performed and Date(s) of Service, including officer(s), employee(s), or Member(s) contacted, for Payment Indicated in Item 11:</p> <p><b>RIDOT BID NO. 7448315 - DESIGN BUILD SERVICES FOR REPLACEMENT OF LAUREL AVE. BR. NO. 397 COVENTRY RI.</b></p> <p>(attach Continuation Sheet(s) SF-LLL-A if necessary)</p>		
<p>15. Continuation Sheet(s) SF-LLL-A attached: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p>		
<p>16. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. This information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.</p>	<p>Signature: </p> <p>Print Name: <b>NEAL LINGORANY</b></p> <p>Title: <b>VICE PRESIDENT</b></p> <p>Telephone No.: <b>401.683.6630</b> Date: <b>04.15.11</b></p>	

Federal Use Only:

Authorized for Local Reproduction  
Standard Form - LLL-A

0011146

**CONFLICTS DISCLOSURE STATEMENT**

RE: RIDOT BID T448315  
LAUREL AVE DESIGN BUIUD

I, NEAL HINGORANY hereby certify as follows:

I am employed as a VICE PRESIDENT of NARRAGANSETT ENGINEERING INC.  
 [TITLE] [COMPANY]  
 and to the best of my knowledge:

**PLEASE CHECK THE APPROPRIATE BOX:**

- I have no family or personal relations currently employed either on a full-time or part-time basis at the Rhode Island Department of Transportation.
- I do have family or personal relations currently employed at the Rhode Island Department of Transportation. Please list their name(s), title(s), and RIDOT Division(s) (if known):

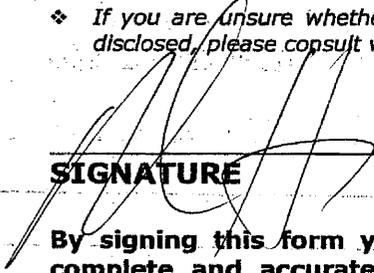
NAME	TITLE	RIDOT DIVISION

*If necessary, please add any additional names as attachments hereto.*

**FOR ILLUSTRATIVE PURPOSES, FAMILY RELATIONS SHALL INCLUDE, WHETHER BY BLOOD, ADOPTION OR MARRIAGE, ANY OF THE FOLLOWING RELATIONSHIPS:**

Father, Mother, Son, Daughter, Brother, Sister, Grandfather, Grandmother, Grandson, Granddaughter, Father-In-Law, Mother-In-Law, Brother-In-Law, Sister-In-Law, Son-In-Law, Daughter-In-Law, Stepfather, Stepmother, Stepson, Stepdaughter, Stepbrother, Stepsister, Half-Brother Or Half-Sister, Niece, Nephew, And Cousin

❖ If you are unsure whether a relationship, association, or connection you have may need to be disclosed, please consult with RIDOT's Legal Office at (401) 222-6510.

  
**SIGNATURE**

04.15.11  
**DATE**

By signing this form you: (1) certify that the information contained in this form is complete and accurate to the best of your knowledge; and (2) acknowledge your continuing obligation to complete and submit a new Disclosure form when there is any change in your family or personal relations during the course of this Contract.

*This document is used for internal RIDOT purposes only in order to address and avoid any potential conflicts at the inception of the contract process and to avoid any impropriety or the appearance of impropriety during the contract process. Any disclosures made hereto will not prejudice prospective vendors from selection.*

Revised: 4/12/2002

**ANTI-COLLUSION CERTIFICATE FOR CONTRACT AND FORCE ACCOUNT  
[Unsworn Declaration]**

Title 23, United States Code, Section 112(c), requires, as a condition precedent to approval by the Director of Public Roads of the contract for this work, that there be filed an unsworn declaration executed by, on behalf of, the person, firm, association, or corporation submitting the bid certifying that such person, firm, association, or corporation has not either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action, in restraint of free competitive bidding in connection with the submitted bid. This unsworn statement shall be in the form of a declaration executed under penalty of perjury under the laws of the United States.

To the: **STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION, DIVISION OF PUBLIC WORKS**

State of RI

County of NEWPORT

I, NEAL HINGORANY, under penalty under the laws of the United States, do depose and say:

On behalf of NARRAGANSETT ENGINEERING of PORTSMOUTH RI that said Contractor has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with Rhode Island Contract Number, Federal-Aid Project Number, County of, Town-City, Road-Bridge.

RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR

CONTRACT/PROPOSAL NAME:

7448315  
DESIGN - BUILD  
LAUREL AVE BRIDGE

NAME OF BIDDER/RESPONDENT:

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor NARRAGANSETT ENGINEERING INC  
Address 3102 E. MAIN RD  
City PORTSMOUTH State RI Zip 02871

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)   
(MSE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

The above named bidder and subcontractor mutually accepts the commitment total estimated for the Unit Prices and Quantities. This commitment total is based on estimated quantities only and most likely will vary up or down as the project is completed. Final compensation and DBE credit will be based on actual quantities of work performed and accepted during the pursuance of work. The listed amount represents the entire dollar amount quoted based on these estimated quantities. No conversations, verbal agreements, and/or other forms of non-written representations shall serve to add, delete, or modify the terms as stated. This document shall not serve in any manner as an actual subcontract between the two parties. A separate subcontractor agreement will describe in detail the contractual obligations of the bidder and the DBE subcontractor.

**Affirmation**

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

NARRAGANSETT ENGINEERING INC

Name of DBE Subcontractor

Name of Bidder

 U.P. 04.15.11  
Signature / Title Date

X \_\_\_\_\_  
Signature / Title Date

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS



Department of Administration  
**Minority Business Enterprise Compliance Office**  
One Capitol Hill  
Providence, RI 02908-5860  
Office: (401) 574-8670  
Fax: (401) 574-8387

RI Relay: 711  
[www.mbe.ri.gov](http://www.mbe.ri.gov)

April 12, 2011

Mr. Kamal Hingorany  
Narragansett Engineering, Inc.  
3102 East Main Road  
Portsmouth, RI 02871

Dear Mr. Hingorany:

Based on the annual review package provided by you, a determination has been made that your firm remains eligible for certification as a MBE for the State of Rhode Island Minority Business Enterprise Program. Your company has been approved as a MBE to conduct business primarily as an **“engineering consulting firm, specializing in structural, site and utility design, surveying, and land development”** firm under primary NAICS Code 541330.

Your certification remains valid until **4/30/2013** unless revoked sooner based on a determination of ineligibility. It is your responsibility to notify the Minority Business Enterprise Compliance Office of any changes in the ownership or control of your business within 30 days of such changes. At the end of your certification period, if you wish to recertify, your company will undergo a substantive review, including a new site visit, as applicable.

In order to maintain your certification during the certification period, you must submit your annual review package sixty (60) days prior to your annual review date which is **4/30/2012**. Your annual review package must include: (a) a completed No Change Affidavit; (b) current corporate financial statements; (c) current corporate and personal tax returns including all schedules and attachments; (d) completed Personal Financial Statement and Statement of Disadvantage Forms; and (e) a copy of your current certification letter from your home state UCP if firm is not based in Rhode Island. Failure to submit your annual review package will result in an administrative removal of your certification.

We wish you success in the State of Rhode Island's Minority Business Enterprise Program; and, if we can be of further assistance to you, please contact this office.

Sincerely,

Charles C. Newton, Administrator  
MBE Compliance Office

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS



Department of Administration  
**Minority Business Enterprise Compliance Office**  
One Capitol Hill  
Providence, RI 02908-5860  
Office: (401) 574-8670  
Fax: (401) 574-8387

RI Relay: 711  
[www.mbe.ri.gov](http://www.mbe.ri.gov)

February 26, 2010

Mr. Kamal Hingorany  
Narragansett Engineering, Inc.  
3102 East Main Road  
Portsmouth, RI 02871

Dear Mr. Hingorany:

Based on the recertification application and supplemental information provided by you, your application for recertification for the State of Rhode Island Minority Business Enterprise Program has been approved. Your company has been approved as a **MBE/DBE** to conduct business primarily as a **“engineering consulting firm, specializing in structural, site and utility design, surveying, and land development”** firm under SIC Code 8711 and under NAICS Code 541330. Your “Minority Business Certification Number” which you can use as proof of your status is MBCN 91. Your company has been identified as a DBE, therefore be advised that the MBE Compliance Office, acting as certification agent for RIDOT, RIAC, and RIPTA, has determined that your firm meets the certification criteria as established by U.S. DOT under 49 CFR Part 26.

Your certification is valid until **3/31/2013** unless revoked sooner based on a determination of ineligibility. It is your responsibility to notify the Minority Business Enterprise Compliance Office of any changes in the ownership or control of your business within 30 days of such changes. At the end of your certification period, if you wish to recertify, your company will undergo a substantive review, including a new site visit, as applicable.

In order to maintain your certification during the certification period, you must submit your annual review package sixty (60) days prior to your annual review date which is **3/31/2011**. Your annual review package must include: (a) a completed No Change Affidavit; (b) current corporate financial statements; (c) current corporate and personal tax returns including all schedules and attachments; (d) completed Personal Financial Statement and Statement of Disadvantage Forms; and (e) a copy of your current certification letter from your home state UCP if firm is not based in Rhode Island. Failure to submit your annual review package will result in an administrative removal of your certification.

We wish you success in the State of Rhode Island’s Minority Business Enterprise Program and the Department of Transportation’s Disadvantaged Business Enterprise Program; and, if we can be of further assistance to you, please contact this office.

Sincerely,

Charles C. Newton, Administrator  
MBE Compliance Office



**STATE BOARD OF REGISTRATION FOR  
PROFESSIONAL LAND SURVEYORS**

DEPARTMENT OF BUSINESS REGULATION  
DIVISION OF DESIGN PROFESSIONALS  
1511 PONTIAC AVENUE  
BUILDING 68-2  
CRANSTON, RI 02920

(401) 462-9595 FAX: (401) 462-9532

[www.bdp.state.ri.us](http://www.bdp.state.ri.us)

**Members of the Board**

Thomas D. Drury, Jr., Chairman  
John Mensinger, Vice Chairman  
Alfred W. DiOrio, Secretary  
Louis Federici  
Richard S. Lipsitz

**Administrative Assistant**

Christina M. Styron

**Legal Counsel**

Louis A. DeQuattro, Jr.

August 4, 2010

KAMAL HINGORANY / NARRAGANSETT ENGINEERING, INC.  
3102 EAST MAIN ROAD

PORTSMOUTH, RI 02871

Dear Sir or Madam:

Your renewal application for a Certificate of Authorization (COA) has been approved. Your COA to practice Land Surveying as defined in Chapter 5-8.1-13. of the General Laws of Rhode Island, was renewed from **June 1, 2010 to May 31, 2012**.

Please find a COA validation sticker attached. This sticker is to be placed in the upper right hand corner of your COA Certificate which was previously issued to you.

If you have any questions concerning this procedure, please contact the staff at this office.

Very truly yours,

Alfred W. DiOrio, PLS  
Secretary

/cms

Sticker Attached



Appendix B--Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower tier Covered Transactions

*Instructions for Certification*

1. By signing and submitting this proposal, the prospective lower tier participant is providing the certification set out below.

2. The certification in this clause is a material representation of fact upon which reliance was placed when this transaction was entered into. If it is later determined that the prospective lower tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

3. The prospective lower tier participant shall provide immediate written notice to the person to which this proposal is submitted if at any time the prospective lower tier participant learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

4. The terms "covered transaction", "debarred", "suspended", "ineligible", "lower tier covered transaction", "participant", "person", "primary covered transaction", "principal", "proposal", and "voluntarily excluded", as used in this clause, have the meanings set out in the Definitions and Coverage sections of rules implementing Executive Order 12549. You may contact the person to which this proposal is submitted for assistance in obtaining a copy of those regulations.

5. The prospective lower tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency with which this transaction originated.

6. The prospective lower tier participant further agrees by submitting this proposal that it will include this clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower

Tier Covered Transaction, "without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

7. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that it is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant may decide the method and frequency by which it determines the eligibility of its principals. Each participant may, but is not required to, check the Nonprocurement List.

8. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

9. Except for transactions authorized under paragraph 5 of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

*Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion—Lower Tier Covered Transactions*

(1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

(2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION,  
AND OTHER RESPONSIBILITY MATTERS  
PRIMARY COVERED TRANSACTIONS**

In accordance with the code of Federal Regulations, Part 49 CFR Section 29.5 10, the prospective primary participant

Maia N. Lima, Bookkeeper, being duly sworn (or executed under penalty of perjury under the laws of the United States), certifies to the best of his/her knowledge and belief, that its principals:

- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily Excluded from covered transactions by any Federal department or agency;
- (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification;
- (d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall list exceptions below.

Exceptions will not necessarily result in denial of award, but, will be considered in determining contractor responsibility. For any exception noted, indicate below to whom it applies, the initiating agency, and the dates of the action. Providing false information may result in criminal prosecution or administrative sanctions. If an exception is noted the contractor must contact the Department to discuss the exception prior to award of the contract.

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## INSTRUCTIONS FOR COMPLETION OF SF-LLL, DISCLOSURE OF LOBBYING ACTIVITIES

This disclosure form shall be completed by the reporting entity, whether subawardee or prime Federal recipient, at the initiation or receipt of a covered Federal action, or a material change to a previous filing, pursuant to title 31 U.S.C. Section 1352. The filing of a form is required for each payment or agreement to make payment to any lobbying entity for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with a covered Federal action. Use the SF-LLL-A Continuation Sheet for additional information if the space on the form is inadequate. Complete all items that apply for both the initial filing and material change report. Refer to the implementing guidance published by the Office of Management and Budget for additional information.

1. Identify the type of covered Federal action for which lobbying activity is and/or has been secured to influence the outcome of a covered Federal action.
2. Identify the status of the covered Federal action.
3. Identify the appropriate classification of this report. If this is a follow-up report caused by a material change to the information previously reported, enter the year and quarter in which the change occurred. Enter the date of the last previously submitted report by this reporting entity for this covered Federal action.
4. Enter the full name, address, city, state and zip code of the reporting entity. Include Congressional District, if known. Check the appropriate classification of the reporting entity that designates if it is, or expects to be a prime or subaward recipient. Identify the tier of the subawardee, e.g., the first subawardee of the prime is the 1st tier. Subawards include but are not limited to subcontracts, subgrants and contract awards under grants.
5. If the organization filing the report in item 4 checks "Subawardee", then enter the full name, address, city, state and zip code of the prime Federal recipient. Include Congressional District, if known.
6. Enter the name of the Federal agency making the award or loan commitment. Include at least one organizational level below agency name, if known. For example, Department of Transportation, United States Coast Guard.
7. Enter the Federal program name or description for the covered Federal action (item 1). If known, enter the full Catalog of Federal Domestic Assistance (CFDA) number for grants, cooperative agreements, loans, and loan commitments.
8. Enter the most appropriate Federal identifying number available for the Federal action identified in item 1 (e.g., Request for Proposal (RFP) number, Invitation for Bid (IFB) number, grant announcement number, the contract, grant, or loan award number, the application/proposal control number assigned by the Federal agency). Include prefixes, e.g., "RFP-DE-90-001".
9. For a covered Federal action where there has been an award or loan commitment by the Federal agency, enter the Federal amount of the award/loan commitment for the prime entity identified in item 4 or 5.
10. (a) Enter the full name, address, city, state and zip code of the lobbying entity engaged by the reporting entity identified in item 4 to influence the covered Federal action.  
  
(b) Enter the full names of the individual(s) performing services, and include full address if different from 10(a). Enter Last Name, First Name and Middle Initial (MI).
11. Enter the amount of compensation paid or reasonably expected to be paid by the reporting entity (item 4) to the lobbying entity (item 10). Indicate whether the payment has been made (actual) or will be made (planned). Check all boxes that apply. If this is a material change report, enter the cumulative amount of payment made or planned to be made.
12. Check the appropriate box(es). Check all boxes that apply. If payment is made through an in-kind contribution, specify the nature and value of the in-kind payment.
13. Check the appropriate box(es). Check all boxes that apply. If other, specify nature.
14. Provide a specific and detailed description of the services that the lobbyist has performed, or will be expected to perform, and the date(s) of any services rendered. Include all preparatory and related activity, not just time spent in actual contact with Federal officials. Identify the Federal official(s) or employee(s) contacted or the officer(s), employee(s), or Member(s) of Congress that were contacted.
15. Check whether or not a SF-LLL-A Continuation Sheet(s) is attached.
16. The certifying official shall sign and date the form, print his/her name, title, and telephone number.

Public reporting burden for this collection of information is estimated to average 30 minutes per response, including time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information. Send comments regarding the burden estimate or any other aspect of this collection of information, including suggestions for reducing this burden, to the Office of Management and Budget, Paperwork Reduction Project (0348-0046), Washington, DC 20503.

**Certification for Federal-Aid Construction/Consultant Contracts**

**IN ACCORDANCE WITH PUBLIC LAW 101-1210 SECTION 319 (DEPARTMENT OF THE INTERIOR AND RELATED AGENCIES) THE PROSPECTIVE PARTICIPANT CERTIFIES, BY SIGNING AND SUBMITTING THIS BID OR PROPOSAL, TO THE BEST OF HIS/HER KNOWLEDGE AND BELIEF, THAT:**

1. No Federal appropriated funds have been paid or will be paid, by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

2. If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract, grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying", in accordance with its instructions.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by Section 1352, Title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

**The prospective participant also agrees by submitting his or her bid or proposal that he or she shall require that the language of this certification be included in all lower tier subcontracts, which exceed \$100,000 and that all such subrecipients shall certify and disclose accordingly.**

**(R.I.D.O.T. APPENDIX C)**

N/A

Complete this form to disclose lobbying activities pursuant to 31 U.S.C 1352 - 0348 - 0046  
(see reverse for public disclosure)

1. Type of Federal Action:	2. Status of Federal Action:	3. Report Type: For Material Change Only: year quarter Date of last report
4. Name and Address of Report Entity: Congressional District, if known:	5. If Reporting Entity in No. 4 is Subawardee, Name and Address of Prime: Congressional District, if known:	
6. Federal Department Agency:	7. Federal Program Name/Description: CFDA Number, if applicable:	
8. Federal Action Number, if known:	9. Award Amount, if known:	
10. a. Name and Address of Lobbying Entity:	10. b. Individuals Performing Services (including address if different from No. 10a)	
11. Amount of Payment (check all that apply) Actual Planned	13. Type of Payment: _ Retainer _ One _ Contingent Fee _ Time Fee _ Deferred _ Commission _ Other; Specify;	
12. Form of Payment: Cash In-kind: specify: Nature: Value:		
14. Brief Description of Services Performed or to be Performed and Date(s) of Service; including officer(s), employee (s), or Member(s) contracted, for Payment indicated in Item 11:		
15. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. this information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.		
For Federal user Only:	Authorized for Local Reproduction Standard Form - LLL	

Reporting Entity: \_\_\_\_\_  
Page \_\_\_\_\_ of \_\_\_\_\_

**DISCLOSURE OF LOBBYING ACTIVITIES**  
**CONTINUATION SHEET**

## **CONFLICTS DISCLOSURE POLICY**

To ensure that the Rhode Island Department of Transportation (RIDOT) maintains the continued confidence and trust of the people of Rhode Island in carrying out its mission, prospective vendors must disclose any family (or other personal) relationships, associations or connections that the vendor, its affiliates, or employees, may currently have with any RIDOT employee. A Conflicts Disclosure Statement shall be submitted to RIDOT from the following:

- ❖ Owners;
- ❖ Directors;
- ❖ Principals;
- ❖ Officers, board members, or individuals with corporate authority;
- ❖ If the vendor is a partnership, the applicant's partners;
- ❖ If the vendor is a limited liability company, its members and managers;
- ❖ Employees with decision-making authority, including executive directors, managers or individuals in a similar position with corporate authority; and
- ❖ Shareholders with a controlling interest.

**CONFLICTS DISCLOSURE STATEMENT**

RE: \_\_\_\_\_  
 \_\_\_\_\_

I, Maria N. Lima hereby certify as follows:

I am employed as a Bookkeeper of M-O-N Landscaping  
 [TITLE] [COMPANY]  
 and to the best of my knowledge:

**PLEASE CHECK THE APPROPRIATE BOX:**

- I have no family or personal relations currently employed either on a full-time or part-time basis at the Rhode Island Department of Transportation.
- I do have family or personal relations currently employed at the Rhode Island Department of Transportation. Please list their name(s), title(s), and RIDOT Division(s) (if known):

NAME	TITLE	RIDOT DIVISION

*If necessary, please add any additional names as attachments hereto.*

**FOR ILLUSTRATIVE PURPOSES, FAMILY RELATIONS SHALL INCLUDE, WHETHER BY BLOOD, ADOPTION OR MARRIAGE, ANY OF THE FOLLOWING RELATIONSHIPS:**

Father, Mother, Son, Daughter, Brother, Sister, Grandfather, Grandmother, Grandson, Granddaughter, Father-In-Law, Mother-In-Law, Brother-In-Law, Sister-In-Law, Son-In-Law, Daughter-In-Law, Stepfather, Stepmother, Stepson, Stepdaughter, Stepbrother, Stepsister, Half-Brother Or Half-Sister, Niece, Nephew, And Cousin

❖ *If you are unsure whether a relationship, association, or connection you have may need to be disclosed, please consult with RIDOT's Legal Office at (401) 222-6510.*

Maria N. Lima  
 SIGNATURE

4-19-11  
 DATE

By signing this form you: (1) certify that the information contained in this form is complete and accurate to the best of your knowledge; and (2) acknowledge your continuing obligation to complete and submit a new Disclosure form when there is any change in your family or personal relations during the course of this Contract.

*This document is used for internal RIDOT purposes only in order to address and avoid any potential conflicts at the inception of the contract process and to avoid any impropriety or the appearance of impropriety during the contract process. Any disclosures made hereto will not prejudice prospective vendors from selection.*

Revised: 4/12/2002

**ANTI-COLLUSION CERTIFICATE FOR CONTRACT AND FORCE ACCOUNT  
[Unsworn Declaration]**

Title 23, United States Code, Section 112(c), requires, as a condition precedent to approval by the Director of Public Roads of the contract for this work, that there be filed an unsworn declaration executed by, on behalf of, the person, firm, association, or corporation submitting the bid certifying that such person, firm, association, or corporation has not either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action, in restraint of free competitive bidding in connection with the submitted bid. This unsworn statement shall be in the form of a declaration executed under penalty of perjury under the laws of the United States.

To the: **STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION, DIVISION OF PUBLIC WORKS**

State of MA

County of Bristol

I, Maria N. Lima, Bookkeeper, under penalty under the laws of the United States, do depose and say:

On behalf of M-D-N Landscaping of Dartmouth MA that said Contractor has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with Rhode Island Contract Number , Federal-Aid Project Number , County of , Town-City , Road-Bridge .

**SUBCONTRACTOR CERTIFICATION – OPTION 2 - ENTITY**

(To be used by an entity (partnership, corporation, limited liability partnership, etc.) doing – or intending to do business with the State of Rhode Island)

**THIS SECTION TO BE COMPLETED BY THE BIDDER/VENDOR**

Vendor Name and ID: «VENDOR\_NAME»  
Purchase Order # \_\_\_\_\_  
RFP # \_\_\_\_\_  
Bid # \_\_\_\_\_

**THIS SECTION TO BE COMPLETED BY THE SUBCONTRACTOR**

Subcontractor Name M-O-N Landscaping  
Address P.O. Box 70220  
City, State Zip NO. Dartmouth MA 02747  
Telephone Number 508-679-3994

I, MARIA LIMA, the BOOKkeeper of M-O-N Landscaping  
(Full name) (Title) (Name of entity – "Entity")

hereby certify that I am a representative of said Entity and am duly authorized to execute this Vendor Certification on behalf of the Entity; that said Entity registered to utilize the E-Verify program on May 13, 2009, 2008 and that the Entity utilizes the services of the E-Verify program to ensure compliance with federal and state requirements. On behalf of the Entity, I understand and agree that the Entity is required to continue to utilize the services of the E-Verify program for as long as the Entity continues to do business with the State of Rhode Island and failure to continue to utilize the services of the E-Verify program will adversely affect the Entity's ability to continue to do business with the State of Rhode Island and will affect the Entity's ability to do business with the State in the future.

ID - 143894

Maria N. Lima Date: 4/19/11  
(Signature)

MARIA N. LIMA  
(Print name)

**NOTARY PUBLIC**

STATE OF Massachusetts  
COUNTY OF Bristol

In N. Dartmouth in said County on the 19<sup>th</sup> day of April, 2008, before me personally appeared

Maria Lima 2011, to me known and known by me to be the party executing the above Vendor Certification on behalf of the Entity, and he/she acknowledged said document, by him/her/ executed to be his/her free act and deed of said Entity.

[Signature]  
(Signature)

(Printed name) MARIO SOUSA My commission expires on: Oct 4<sup>th</sup>, 2013

Complete and Return To: State of Rhode Island, Division of Purchases, P O. Box 41544, Providence, Rhode Island 02940

**RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR**

CONTRACT/PROPOSAL NAME:

NAME OF BIDDER/RESPONDENT:

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor M-O-N Landscaping  
Address P.O. Box 70220  
City No. Dartmouth State MA Zip 02747

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

The above named bidder and subcontractor mutually accepts the commitment total estimated for the Unit Prices and Quantities. This commitment total is based on estimated quantities only and most likely will vary up or down as the project is completed. Final compensation and DBE credit will be based on actual quantities of work performed and accepted during the pursuance of work. The listed amount represents the entire dollar amount quoted based on these estimated quantities. No conversations, verbal agreements, and/or other forms of non-written representations shall serve to add, delete, or modify the terms as stated. This document shall not serve in any manner as an actual subcontract between the two parties. A separate subcontractor agreement will describe in detail the contractual obligations of the bidder and the DBE subcontractor.

**Affirmation**

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

M-O-N Landscaping, Inc

Name of DBE Subcontractor

Name of Bidder

[Signature]

Signature / Title

Date

Signature / Title

Date

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION,  
AND OTHER RESPONSIBILITY MATTERS  
PRIMARY COVERED TRANSACTIONS**

In accordance with the code of Federal Regulations, Part 49 CFR Section 29.5 10, the prospective primary participant

Kathleen Gorman, Schedule Dynamics, Inc., being duly sworn (or executed under penalty of perjury under the laws of the United States), certifies to the best of his/her knowledge and belief, that its principals:

- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily Excluded from covered transactions by any Federal department or agency;
- (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (1)(b) of this certification;
- (d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall list exceptions below.

Exceptions will not necessarily result in denial of award, but, will be considered in determining contractor responsibility. For any exception noted, indicate below to whom it applies, the initiating agency, and the dates of the action. Providing false information may result in criminal prosecution or administrative sanctions. If an exception is noted the contractor must contact the Department to discuss the exception prior to award of the contract.

*Kathleen Gorman*

4-14-11

**CONFLICTS DISCLOSURE STATEMENT**

RE: DESIGN/BUILD services for the Replacement of the Laurel Avenue  
Br. No 397

I, Kathleen Gorman hereby certify as follows:

I am employed as a President of Schedule Dynamics, Inc  
 [TITLE] [COMPANY]

and to the best of my knowledge:

**PLEASE CHECK THE APPROPRIATE BOX:**

I have no family or personal relations currently employed either on a full-time or part-time basis at the Rhode Island Department of Transportation.

I do have family or personal relations currently employed at the Rhode Island Department of Transportation. Please list their name(s), title(s), and RIDOT Division(s) (if known):

NAME	TITLE	RIDOT DIVISION

*If necessary, please add any additional names as attachments hereto.*

**FOR ILLUSTRATIVE PURPOSES, FAMILY RELATIONS SHALL INCLUDE, WHETHER BY BLOOD, ADOPTION OR MARRIAGE, ANY OF THE FOLLOWING RELATIONSHIPS:**

Father, Mother, Son, Daughter, Brother, Sister, Grandfather, Grandmother, Grandson, Granddaughter, Father-In-Law, Mother-In-Law, Brother-In-Law, Sister-In-Law, Son-In-Law, Daughter-In-Law, Stepfather, Stepmother, Stepson, Stepdaughter, Stepbrother, Stepsister, Half-Brother Or Half-Sister, Niece, Nephew, And Cousin

❖ *If you are unsure whether a relationship, association, or connection you have may need to be disclosed, please consult with RIDOT's Legal Office at (401) 222-6510.*

Kathleen Gorman 4-14-11  
 SIGNATURE DATE

By signing this form you: (1) certify that the information contained in this form is complete and accurate to the best of your knowledge; and (2) acknowledge your continuing obligation to complete and submit a new Disclosure form when there is any change in your family or personal relations during the course of this Contract.

*This document is used for internal RIDOT purposes only in order to address and avoid any potential conflicts at the inception of the contract process and to avoid any impropriety or the appearance of impropriety during the contract process. Any disclosures made hereto will not prejudice prospective vendors from selection.*

Complete this form to disclose lobbying activities pursuant to 31 U.S.C 1352 - 0348 - 0046  
(see reverse for public disclosure)

1. Type of Federal Action:	2. Status of Federal Action:	3. Report Type:  For Material Change Only: year quarter Date of last report
4. Name and Address of Report Entity:  Congressional District, if known:		5. If Reporting Entity in No. 4 is Subawardee, Name and Address of Prime:  Congressional District, if known:
6. Federal Department Agency:	7. Federal Program Name/Description:  CFDA Number, if applicable:	
8. Federal Action Number, if known:	9. Award Amount, if known:	
10. a. Name and Address of Lobbying Entity:	10. b. Individuals Performing Services (including address if different from No. 10a)	
11. Amount of Payment (check all that apply)  Actual Planned	13. Type of Payment:  _ Retainer    _ One    _ Contingent Fee _ Time Fee    _ Deferred _ Commission    _ Other; Specify;	
12. Form of Payment:  Cash  In-kind: specify: Nature: Value:		
14. Brief Description of Services Performed or to be Performed and Date(s) of Service; including officer(s), employee (s), or Member(s) contracted, for Payment indicated in Item 11: <i>No lobbying services are being provided.</i>		
15. Information requested through this form is authorized by title 31 U.S.C. section 1352. This disclosure of lobbying activities is a material representation of fact upon which reliance was placed by the tier above when this transaction was made or entered into. This disclosure is required pursuant to 31 U.S.C. 1352. This information will be reported to the Congress semi-annually and will be available for public inspection. Any person who fails to file the required disclosure shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.		
For Federal user Only:	Authorized for Local Reproduction Standard Form - LLL	

*Kathleen Gorman*

Revised: 4/12/2002

**ANTI-COLLUSION CERTIFICATE FOR CONTRACT AND FORCE ACCOUNT  
[Unsworn Declaration]**

Title 23, United States Code, Section 112(c), requires, as a condition precedent to approval by the Director of Public Roads of the contract for this work, that there be filed an unsworn declaration executed by, on behalf of, the person, firm, association, or corporation submitting the bid certifying that such person, firm, association, or corporation has not either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action, in restraint of free competitive bidding in connection with the submitted bid. This unsworn statement shall be in the form of a declaration executed under penalty of perjury under the laws of the United States.

To the: **STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF TRANSPORTATION, DIVISION OF PUBLIC WORKS**

State of MA

County of Middlesex

I, Kathleen Gorman, Schedule Dynamics Inc., under penalty under the laws of the United States, do depose and say:

On behalf of Schedule Dynamics Inc., of Sudbury, MA that said Contractor has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with Rhode Island Contract Number, Federal-Aid Project Number, County of, Town-City, Road-Bridge.

RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR

CONTRACT/PROPOSAL NAME:

NAME OF BIDDER/RESPONDENT:

DESIGN/BUILD Services for the  
Replacement of the Laurel Avenue  
Br. No. 397

A.A. Will Corporation

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor Schedule Dynamics, Inc.  
Address 15 Wake Robin Rd., Sudbury, MA, 01776  
City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

The above named bidder and subcontractor mutually accepts the commitment total estimated for the Unit Prices and Quantities. This commitment total is based on estimated quantities only and most likely will vary up or down as the project is completed. Final compensation and DBE credit will be based on actual quantities of work performed and accepted during the pursuance of work. The listed amount represents the entire dollar amount quoted based on these estimated quantities. No conversations, verbal agreements, and/or other forms of non-written representations shall serve to add, delete, or modify the terms as stated. This document shall not serve in any manner as an actual subcontract between the two parties. A separate subcontractor agreement will describe in detail the contractual obligations of the bidder and the DBE subcontractor.

**Affirmation**

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

Schedule Dynamics, Inc.

Name of DBE Subcontractor

Name of Bidder

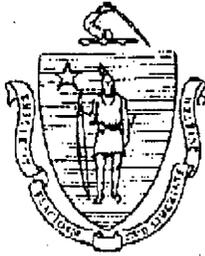
Kathleen Sorwein/President 4-14-11

Signature / Title

Date

Signature / Title

Date



**COMMONWEALTH OF MASSACHUSETTS**  
**DEPARTMENT OF BUSINESS AND TECHNOLOGY**  
**STATE OFFICE OF MINORITY AND WOMEN BUSINESS ASSISTANCE**

Massachusetts Transportation Building  
Ten Park Plaza, Suite 3740, Boston, MA 02116

[www.mass.gov/somwba](http://www.mass.gov/somwba)

DEVAL L. PATRICK  
GOVERNOR

TIMOTHY P. MURRAY  
LIFUTENANT GOVERNOR

REGINALD A. NUNNALLY  
EXECUTIVE DIRECTOR

TELEPHONE:  
(617) 973-8692

FACSIMILE:  
(617) 973-8637

October 20, 2009

Ms. Kathleen Gorman  
Schedule Dynamics, Inc.  
15 Wake Robin Road  
Sudbury, MA 01776

Dear Ms. Gorman:

The State Office of Minority and Women Business Assistance (SOMWBA), acting as certification agent for the Massachusetts Highway Department (MHD), the Massachusetts Bay Transportation Authority (MBTA), the Massachusetts Port Authority (MassPORT), the Massachusetts Turnpike Authority/Central Artery/Tunnel (MTA/CAT) and the Massachusetts Aeronautics Commission (MAC) is pleased to notify you that your company has been renewed as a disadvantaged business enterprise (DBE). Your company continues to be assigned NAICS Code(s) 236220.

Your firm's designation as a DBE will appear in the Massachusetts Central Register and the DBE Certified Business Listing of eligible firms for federal-aid transportation projects.

As a DBE, you must inform SOMWBA in writing of any change in circumstances affecting your ability to meet size, disadvantaged status, ownership, control requirements or any material change in the information provided in your application form. Changes in management responsibility among members of a limited liability company are covered by this requirement. You must attach supporting documentation describing in detail the nature of such changes. The notice must take the form of an affidavit sworn to by the owners of the firm before a person who is authorized by state law to administer oaths or of an un-sworn declaration executed under penalty of perjury of the laws of the United States. You must provide the written notification within 30 days of the occurrence of the change. If you fail to make timely notification of such a change, you will be deemed to have failed to cooperate under 49 CFR 26.109(c).

To renew your firm's DBE certification and if it continues to meet the applicable criteria, on or before your firm's certification anniversary date of November 2, 2010, and each year thereafter, please send SOMWBA the following documents:

- (1) Please complete the attached No Change Affidavit. SOMWBA will provide the affidavit in reminder letter annually.
- (2) A signed copy of your company's, and all of its affiliates', U.S. Tax Returns including all schedules and attachments for the year(s) indicated.

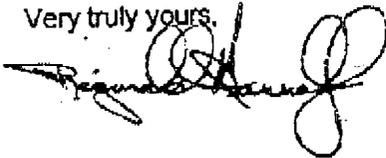
- (3) Sole proprietors, a signed copy of your complete tax return including the Schedule C, for the year(s) indicated.
- (4) Personal Tax Returns for the last two (2) years.
- (5) All company financial statements for the year(s) indicated.
- (6) A notarized statement of the number of full-time and part-time employees (including owner) for each year indicated.
- (7) Completed Personal Financial Statement and Statement of Disadvantage Forms.

If you have changed your company name or address, please notify Ms. Nedra D. White, in writing on the company's letterhead in order to update your state vendor file.

SOMWBA reserves the right to monitor, perform random spot checks, re-evaluate the firm or revoke the firm's certification if it no longer meets the certification criteria.

During the period of your certification, if you have further questions regarding annual reviews, please contact Ms. Nedra D. White, SOMWBA/DBE Renewal Specialist, at (617) 973-8648.

Very truly yours,



Reginald A. Nunnally  
Executive Director

RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR

CONTRACT/PROPOSAL NAME:

NAME OF BIDDER/RESPONDENT:

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor Atlantic Bridge & Engineering, Inc.  
Address 191 Elm Street No. 7  
City Salisbury State MA Zip 01952

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

The above named bidder and subcontractor mutually accepts the commitment total estimated for the Unit Prices and Quantities. This commitment total is based on estimated quantities only and most likely will vary up or down as the project is completed. Final compensation and DBE credit will be based on actual quantities of work performed and accepted during the pursuance of work. The listed amount represents the entire dollar amount quoted based on these estimated quantities. No conversations, verbal agreements, and/or other forms of non-written representations shall serve to add, delete, or modify the terms as stated. This document shall not serve in any manner as an actual subcontract between the two parties. A separate subcontractor agreement will describe in detail the contractual obligations of the bidder and the DBE subcontractor.

**Affirmation**

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

Atlantic Bridge & Engineering  
Name of DBE Subcontractor

A.A. Will Corporation  
Name of Bidder

[Signature] Treasurer 4/20/11  
Signature / Title Date

[Signature] Vice President 4/20/11  
Signature / Title Date

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS



Department of Administration  
**Minority Business Enterprise Compliance Office**  
One Capitol Hill  
Providence, RI 02908-5860  
Office: (401) 574-8670  
Fax: (401) 574-8387

RI Relay: 711  
[www.mbe.ri.gov](http://www.mbe.ri.gov)

December 7, 2010

Ms. Victoria Kolenda  
Atlantic Bridge & Engineering, Inc.  
191 Elm Street  
Salisbury, MA 01952

Dear Ms. Kolenda:

Based on the recertification application and supplemental information provided by you, your application for recertification for the State of Rhode Island's MBE Program and/or the U.S. Department of Transportation's DBE Program has been approved. Your company has been approved as a **WBE/DBE** to conduct business primarily as a "installation of structural steel and related work for buildings and bridges, precast beams, misc. metals and rebar; fabrication of misc. metals and structural steel for bridges and buildings" firm under primary NAICS Code 238120. Your "Minority Business Certification Number" which you can use as proof of your status is MBCN 1146. If your company has been identified as a DBE, be advised that the MBE Compliance Office, acting as certification agent for RIDOT, RIAC, and RIPTA, has determined that your firm meets the certification criteria as established by U.S. DOT under 49 CFR Part 26.

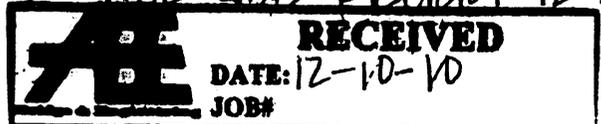
Your certification is valid until **12/31/2013** unless revoked sooner based on a determination of ineligibility. It is your responsibility to notify the Minority Business Enterprise Compliance Office of any changes in the ownership or control of your business within 30 days of such changes. At the end of your certification period, if you wish to recertify, your company will undergo a substantive review, including a new site visit, as applicable.

In order to maintain your certification during the certification period, you must submit your annual review package sixty (60) days prior to your annual review date which is **12/31/2011**. Your annual review package must include: (a) a completed No Change Affidavit; (b) current corporate financial statements; (c) current corporate and personal tax returns including all schedules and attachments; (d) completed Personal Financial Statement and Statement of Disadvantage Forms; and (e) a copy of your current certification letter from your home state UCP if firm is not based in Rhode Island. Failure to submit your annual review package will result in an administrative removal of your certification.

We wish you success in the State of Rhode Island's MBE Program and/or the Department of Transportation's DBE Program, and if we can be of further assistance to you, please contact this office.

Sincerely,

Charles C. Newton, Administrator  
MBE Compliance Office



RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR

CONTRACT/PROPOSAL NAME:

NAME OF BIDDER/RESPONDENT:

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor FC CONSTRUCTION  
Address PO BOX 1630  
City WESTPORT State MA Zip 02790

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

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Affirmation

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

FC CONSTRUCTION  
Name of DBE Subcontractor

AA Will Corporation  
Name of Bidder

[Signature]  
Signature / Title  
Date  
4/19/11

[Signature]  
Signature / Title  
Date  
4/21/11

RHODE ISLAND DEPARTMENT OF TRANSPORTATION  
LETTER OF INTENT TO PERFORM AS A SUBCONTRACTOR

CONTRACT/PROPOSAL NAME:

NAME OF BIDDER/RESPONDENT:

The undersigned intends to perform work in connection with the above contract upon the execution and subsequent award of the contract by RIDOT:

Name of DBE Subcontractor D&M Concrete Floor Co.  
Address 7 Park Street  
City Fall River State MA. Zip 02721

Please check if RI certification letter attached:

Disadvantaged Business Enterprise (DBE)

The DBE status of the above named subcontractor is certified by the Rhode Island Department of Administration. The above named subcontractor is prepared to perform the described work listed on the DBE Utilization Plan, in connection with the above contract upon execution of such and subsequent award of the contract by RIDOT. The above named subcontractor is prepared to perform the described work for the Price identified on the DBE Utilization Plan. (See Price Proposal).

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**Affirmation**

The above named DBE subcontractor affirms that it will perform the portion(s) of the contract for the estimated dollar value as stated in the DBE Utilization Plan (see Price Proposal).

D&M Concrete Floor Co.  
Name of DBE Subcontractor

A.A. Will Corporation  
Name of Bidder

Joseph M... 4/21/11  
Signature / Title Date

[Signature] 4/21/11  
Signature / Title Date

# JOHN F. McNAMARA

## VICE PRESIDENT

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### EXPERTISE

Mr. McNamara offers 34 years of construction management expertise, specifically with large scale public infrastructure projects throughout New England and the Northeastern United States.

### EXPERIENCE

**A.A. Will Corporation, Stoughton, MA** **2009-Present**  
**Vice President**-Responsible for planning, estimating and directing general contracting work on public work projects.

- Services include: transit, highway, bridge construction, demolition, utility work, marine construction, cut and cover tunnels, underpinning, drilled shafts, design and installation of lateral earth support systems, slurry walls, environmental and solid waste landfill work, site remediation including excavation, transportation and disposal of both hazardous and non hazardous materials, and General Contracting Turnkey Design Build Projects.

**Minot Coastal Company, New York** **2005-2008**  
**Owner & Operator** of Marine logistics firm, which transported recycled aggregate via barge from metropolitan NY to the Albany, NY area.

**Modern Continental Construction Co., Inc.** **1982-2005**  
**Senior Vice President** (1992-2005), John led the firm I-90 East and South Boston Office (1992-2000) for Central Artery/Tunnel Projects as well as NY regional office (1998-2005) • **Vice President** (1988-1992) • **Project Manager** (1983-1988) • **Superintendent** (1982-1983)

### RELEVANT PROJECTS

**Fountain Ave Landfill, Brooklyn, NY, \$150M.** Closure of 300 acre landfill with polyethylene liner, utilizing 4M CY of material delivered via barge to a marine facility designed and constructed on site; work also included: roadways, drainage and site utilities.

**Long Island Expressway, Queens, NY, \$150M.** 7 mile reconstruction of I-495 from the Mid Town Tunnel to the Grand Central Parkway; work included: phased demolition and replacement of 17 bridge structures, retaining walls, full depth roadway reconstruction, slip form paving, and ITS installation.

**I-90 Fort Point Channel Immersed Tube Tunnel, Cast in Place Tunnels, & Subaru Pier Materials Transfer Facility, South Boston, MA, Combined value \$420M.** Construction of 6 concrete immersed tube elements (40,000-70,000T ea.), placed on drilled shaft foundations over MBTA redline within the Fort Point Channel, and construction of 2000 LF of cut and cover cast in place tunnel. Work included 400,000 CY of earth excavation, 300,000 CY dredging, 350,000 CY of cast in place concrete, 50,000T of reinforcing steel, construction of circular cofferdams, slurry wall tied back earth support, and construction of 2 bridge structures over the Fort Point Channel.

**I-93/I-90 Interchange Ramps, South Boston, MA, \$160M.** Demolition and reconstruction of the I-93 Southbound viaduct utilizing precast segmental concrete and the extension of I-90 east and west bound boat sections with multiple traffic detours and temporary bridge structures over active rapid transit, commuter rail and freight tracks without causing disruption to service.

**Bird Island Flats Tunnel (Ted Williams Tunnel I-90), East Boston, MA, \$300M.** Constructed 2000 LF of cut and cover cast in place concrete tunnel and 1000 LF of cast in place concrete boat section (I-90) through Logan International Airport utilizing

600,000 square feet of soil mix and slurry wall tied back with tunnel portal and toll plaza facility. Work included: 1.2 million cubic yards of excavation, placing 500,000 CY of cast in place concrete with 50,000T of reinforcing steel, relocation of existing airport taxiway for Logan Terminal A, bridge structures backfill of tunnel, and restoration of taxiway.

#### **EDUCATION**

**B.S. Civil Engineering, Northeastern University, Boston, MA 1981**

#### **ACCREDITATIONS**

MA Licensed Construction Supervisor • MA Certification for Blasting Operations

#### **AFFILIATIONS**

**Moles Member 1994-Present**  
**Finance Committee** 2006-Present. Also served as Secretary (2002-05), Executive Committee (2000-05), and Trustee (2000-01).

**General Contractors Association of New York 1999-2005**  
**Executive Committee**

**Construction Industries of Massachusetts 1995-2002**  
Served as Chairman (1996), Board of Directors (1991-97), **and** Labor Relations Executive Committee (1995-2002).

**American Road and Transportation Builders Association 1995-1999**  
**Board of Directors**

**American Society of Civil Engineers 1981-Present**

**Boston Society of Civil Engineers 1981-Present**

**American Society of Military Engineers 1994-Present**

## **WILLIAM E. ZANIBONI**

### **PROJECT MANAGER & ESTIMATOR**

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#### **EXPERTISE**

Mr. Zaniboni has been with A.A. Will for 46 years and offers experience in the construction field including site supervision, project management, estimating, and quality control for construction of roads, bridges, solid waste landfills, utilities, deep foundations, earth support, underpinning, retaining walls, soil bentonite and cement bentonite cut off slurry walls, dynamic compaction and environmental remediation projects throughout the Northeast.

#### **EXPERIENCE**

**A.A. Will Corporation, Stoughton, MA** **1964-Present**  
**Project Manager & Estimator** **1985-Present**  
Responsibilities include: solicitation, quantity takeoff, pricing, negotiation, purchasing, scheduling, and coordination of heavy/civil and building construction projects. Also served as **Project Engineer, Superintendent & Foreman** (1971-84), **Assistant to Engineers**, and **Equipment Operator & Laborer** (1964-71).

#### **RELEVANT PROJECTS**

**New Bedford Freight Railroad Bridge**, New Bedford, MA, 2011, **\$16.4M.**  
**Hanover/Hanson Bridge Rehabilitation**, Hanover/Hanson, MA, 2010, **\$1.4M.**  
**Dighton/Taunton Bridge Replacement**, Dighton/Taunton, MA, 2009, **\$1.2M.**  
**West End Residences**, Charles River Park, MA, 2008, **\$12.8M.**  
**Boston University Medical Center Building F**, Boston, MA, 2008, **\$5.8M.**  
**Patriot Place**, Foxboro, MA, 2007, **\$12.2M.**  
**Blue Cross Blue Shield Headquarters**, Hingham, MA, 2006, **\$9M.**  
**Biogen IDEC Building 6A**, Cambridge, MA, 2006, **5.7M.**  
**MIT Brain and Cognitive Sciences Project**, Cambridge, MA, 2006, **\$2.6M.**  
**World Trade Center East and West**, Boston, MA, 2002, **\$16.3M.**  
**One Lincoln Street**, Boston, MA, 2001, **\$15.1M.**  
**Reebok World Headquarters**, Canton, MA, 2000, **5.9M.**  
**CA/T Contract C19E5 Leverett Circle**, Boston, MA, 1999, **\$13.8M.**  
**Commonwealth Landfill**, Minersville, PA, 1997, **\$14.5M.**

#### **EDUCATION**

**B.A. Sociology, Southern Connecticut State University, Newhaven, CT 1971**

#### **ACCREDITATIONS**

Certificate of completion: 40 hour Hazardous Waste training in accordance with 29 CFR 1910.120

# MICHAEL RENNIE

## PROJECT MANAGER

---

### EXPERTISE

Mr. Rennie offers 20 years of experience in construction in both the public and private sector. As Construction Manager, he is responsible for all on site operations, including engineering, safety and quality control.

### EXPERIENCE

#### **A.A. Will Corporation, Stoughton, MA**

**2010 – Present**

**Project Manager.** Relevant projects include:

**MBTA, New Bedford Bridge Rehabilitation, 2011, \$16.4M.** Rehabilitation of 3 bridges over Dean Street, Sawyer Street, and Coggeshall Street, New Bedford, MA.

#### **LM Heavy Civil Construction, LLC, Westford, MA**

**2008-2010**

**Project Manager.** Managed various civil construction projects including MassDOT bridge replacement subcontracts in Mansfield and New Bedford, MA.

Responsibilities include tracking quantities and revenues, coordination of daily field operations, scheduling, and managing subcontractors, purchasing, pricing and negotiating various change orders. Work closely with project owner's to successfully complete the jobs safely and on time.

#### **J. Derenzo Co., Brockton, MA**

**2006-2008**

**Project Manager.** Managed multiple site work contracts for private and public developments ranging in size from \$2 million to \$15 million. Responsibilities included; project start-up, buyouts, coordination of daily field operations, scheduling, managing subcontractors, tracking and projecting costs and revenues and pricing and negotiating change orders.

#### **Modern Continental, Boston, MA**

**1992-2006**

**Superintendent.** Participated in project start-up, buyouts, subcontracts and scheduling. Supervised the daily activities of multiple trades, which included utility installations, tunnel construction, removal of the existing elevated artery and final surface restorations with emphasis on safety and meeting the vigorous schedules, projected revenues and controlling daily costs. Key projects include: Central Artery Tunnel C17A9, \$379M and Central Artery Tunnel C17A6, \$417M.

#### **Project Engineer/Superintendent**

**1997-1998**

Participated in project start-up and buyouts for all aspects of the job. Supervised the MBTA Redline Tunnel modifications for the successful float-out of the Immersed Tube Tunnel sections. Key project include MassDOT, Central Artery C09B1, \$301M.

#### **Estimator**

**1996-1997**

Worked on various take-offs for numerous construction proposals. Additional responsibilities included: solicited and negotiated subcontractor and vendor proposals for upcoming bids and part of the estimating team which successfully won \$1.5 billion worth of contracts..

#### **Project Engineer**

**1992-1996**

Maintained an extensive project submittal program, tracked daily field quantities and coordinated mechanical and electrical subcontractor's daily tasks. Key project as a P.E., include: MassDOT, Bird Island Flats Tunnel, Central Artery Contract, \$300M.

### Education

B.S., Construction Management, Wentworth Institute of Technology, Boston, MA **1994**

### Training

OSHA 10, OSHA 30, HAZWOPER 40

## GLENN MILSON

### PROJECT SUPERINTENDENT

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#### EXPERTISE

Mr. Milson offers over 30 years experience in the construction field, site supervision on road, bridge, and site development project. As well as site operator, laborer and experience performing heavy civil construction projects throughout New England.

#### EXPERIENCE

**A.A. Will Corporation, Stoughton, MA** **2002 – Present**

**Project Superintendent.** Responsible for complete site supervision, scheduling coordination of workforce, subcontractors and multiple trades. Supervised the daily activities

**Equipment Operator/Laborer.** Worked as operator and laborer gaining critical job training as a project superintendent, responsibilities included: operator, specifically pay loader/backhoe operator and laborer.

#### KEY PROJECTS INCLUDE

- **Dighton-Taunton Bridge Rehabilitation, Dighton/Taunton, MA, 2009. \$1.2M**
- **Patriot Place, Foxboro, MA, 2007. \$14.5M**
- **Four Point Sheraton, Norwood, MA 2006. \$5.9M**
- **Rhode Island Hospital Bridge Building, Providence, RI, 2005. \$3.6M**
- **Rand Whitney Realty LLC, Montville, CT, 2004. \$797,073**
- **Foxboro Stadium Infrastructure, Foxboro, MA, 2004. \$23.7M.**

#### Education

Cranston West High School, Cranston, RI 1970

#### Accreditations

OSHA 10-Hour Occupational Safety and Health Training Course in Construction Safety & Health; CPR & AED/Child CPR & AED/ Infant CPR; Hoisting Engineer License; Payloader/Backhoe Certification; AMTRACK OSHA Compliance

**ELIZABETH WILL**  
**CORPORATE RISK MANAGER**

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**EXPERTISE**

Ms. Will leads A.A. Will's proactive approach to safety and offers over 20 years of experience in Corporate Risk Management, including implementing the corporate wide Health and Safety Program, managing OSHA compliance and Site Safety Officer.

**EXPERIENCE**

**A.A. Will Corporation, Stoughton, MA**

**1987- Present**

**Corporate Risk Manager/Site Safety Officer** responsible for identifying, assessing, and prioritizing potential risk and resource coordination to minimize, monitor, and eliminate the potential hazard. Duties include:

- Pre-construction site visits to assess field conditions, such as location and identification of utilities, review permits, verification of alignments, right of way restrictions or easements
- Maintains work-in-progress safety through various methods, such as conducting site safety inspections, Tool Box Talks, and maintaining up-to-date safety policies and regulations
- Continuously monitor workers for exposure to safety or health hazardous conditions.
- Project wrap up
- Corporate Health & Safety Program, ensuring compliance with the Corporate Office, employees as well as sub-contractors
- Employee training
- Workers' compensation claim management

**OSHA Compliance Manager** responsible for effectively preparing sites for OSHA inspections, reducing and eliminating citations, manage claims processing and provide employee training to ensure compliance. Duties include:

- Manage and contest OSHA citations received
- Conduct OSHA compliance site inspections
- Provide education and training on OSHA standards and manage corporate-wide accountability for compliance to standards
- Accompany OSHA inspectors during site inspection

**EDUCATION**

**Wentworth Institute, Boston, MA**

**1985**

**Northeastern University, Boston, MA**

**1985**

**ACCREDITATIONS**

NCSH #214 Mobile Crane Hazards • #7300 Introduction OSHA Permit-Required Confined Space Standard Trained • Double-Wall Tank Sump Installation Technician • OSHA 10 • OSHA 30 • Turner OSHA 30 • 40 Hour Hazardous Waste Training • CPR/AED Trained • IIFE Trained with Bovis and Skanska

**QUALIFICATIONS**

Incident Investigations • Confined Space • Trench Excavation • Fall Protection • Odor Mitigation • Insurance Claims • On-site experience • Implementation of Health and Safety Program • Site Specific Plans • Experience with a variety of gas detection products, dust monitoring and engineered controls • Member of the Construction Safety Round Table of Eastern MA • Steering Committee Member from 2003



# Yihui Peter Wu, PhD, PE

## Design Manager

MAGUIRE GROUP INC.  
PROJECT MANAGER/PRINCIPAL  
ENGINEER

### TECHNICAL EXPERTISE

- Highway and railway bridge design and construction
- Structure design and analysis
- Seismic analysis and design

### YEARS OF EXPERIENCE

Maguire: Since 2001  
Total: Since 1992

### EDUCATION

Post-Doctoral,  
Structural Engineering,  
SUNY at Buffalo, 1997  
Doctor of Engineering, China Academy  
of Railway Sciences, 1992  
Master of Science of Civil Engineering,  
Southwestern Jiaotong University,  
China, 1986  
B.S. Civil Engineering, South Western  
Jiaotong University, China, 1983

### PROFESSIONAL REGISTRATIONS

Professional Engineer:  
Structural (FL, MA, NH, RI)  
NHI Certificate of Safety Inspection of  
In-Service Bridge  
MBCR Roadway Worker Protection  
Training  
MBTA Right of Way  
Safety Training  
OSHA 10-hour Construction Safety  
Certification

### PROFESSIONAL ASSOCIATIONS

Member of EERI (Earthquake  
Engineering Research Institute)  
Member of ASCE (American Society of  
Civil Engineers), BSCE (Boston Society  
of Civil Engineers for Contractors)

### PROFESSIONAL PROFILE

Dr. Wu has over 17 years experience in structural engineering design, research and teaching. His experience and expertise are bridge design and construction, management of transportation project, seismic analysis, design and retrofit for bridge, railway track and structure design, non-linear and FEM analysis of complicated structure. He has been the project manager for MassDOT bridge design and construction projects, and project manager for RIODT transportation project. He has been selected for peer review of FHWA "Guidelines for Seismic Retrofitting of Steel Truss Highway Bridges-2005". Dr. Wu has authored/coauthored 25 technical papers which have been published in journals and conference proceedings.

### REPRESENTATIVE PROJECTS

**RIDOT, Providence River Bridge (the Iway Bridge), I-195 Relocation Project, Providence, RI:** Senior Engineer for the bridge seismic modeling, analysis, the steel network arch span detail design (connection, seismic restrain, knuckle end), drilled shaft foundation design, pier and H-pile foundation design, and abutment design. Bridges are supported on drilled shaft, H-pile and spread footing foundations. The \$85-million 1,265-ft Providence River Bridge includes 2 steel box girder spans, the 400-ft network arch main span, and 4 pre-cast concrete box beam spans. The network arch span is the widest among its kind; its superstructure steel weighs ~6 million pounds and was assembled off site and then floated up Narragansett Bay into position. The barge-based moving of the superstructure was documented by The History Channel and shown on its *Mega Movers* cable show. This project is the signature bridge for the \$460-million I-195 relocation and improvement project. Its seismic design is based on the 2,500-year return period earthquake. The design and construction seismic and detail design were presented at the 25<sup>th</sup> International Bridge Conference in Pittsburgh, PA in June 2008.

**RIDOT, I-195 Relocation Project, 11 Bridges / LRFR Rating, Providence, RI:** Project Manager/Principal Engineer for managing, QA/QC control of the I-195 project's new bridge LRFR ratings. Eleven newly constructed bridges along with and 2 existing bridges were rated per AASHTO LRFR standard using VIRTIS (BRASS Engine), SAP 2000, and DESCUS programs. Rated bridge superstructure types include network steel arch bridge, curved steel tub girder, curved I girder, prestressed box concrete beam, prestressed bulb beams, cast-in-place reinforced-concrete rigid frame, and multi-cell reinforced-concrete box beam.

**RIDOT, I-195 Relocation Project, Ramp NE Bridge, Providence, RI:** Principal Engineer for bridge design from 60% design to finish; supervision of bridge design; coordination of highway, geotech and architectural design; and the design schedule. Responsible for bridge seismic analysis and modeling, girder field splice and shear studs design, concrete pier and H-pile foundation design, supervision of drafting, estimates, and construction specifications. Ramp NE was one of the \$64-million/Contract 6 bridges with a 1,860-ft 8-span curved steel box girder. Ramp NE is supported on H pile foundation; its hammer pier cap beam is post-tensioned. This bridge connects I-95 north to the new I-195 east. Its earthquake design is based upon a 2,500-year return period.



## **Yihui Peter Wu, PhD, PE** *Design Manager*

**RIDOT, I-195 Relocation Project, Ramp WS Bridge, Providence, RI:** Principal Engineer in charge of bridge design from 60% design to finish; supervision of bridge design; coordination of highway, geotech and architectural design; and the design schedule. Responsible for bridge seismic analysis and modeling, dead and live load analysis and modeling, box girder design, girder field splice and shear studs design, concrete pier and H-pile foundation design, integral pier beam design, and supervision of drafting, estimates and construction specifications. Also responsible for bridge construction coordination, shop drawing reviews/approval, and RFIs. Ramp WS is one of the \$64-million/Contract 6 bridges with a 1,916-ft 11-span curved steel box girder. Ramp WS is supported on an H-pile foundation; its hammer pier cap beam is post-tensioned. This bridge connects the new I-195 west to I-95 south. Its seismic design is based on a 2,500-year return period earthquake.

**RIDOT, I-195 Relocation Project, Ramp SE Bridge, Providence, RI:** Principal Engineer for design criteria and analysis methodology, supervision of bridge seismic and static loading analysis, coordination of steel box girder and detail design, bridge construction coordination, shop drawing approval, and RFIs. Ramp SE is a 10-span, 1,613-ft curved steel box girder, supported on an H-pile foundation with one integral pier crossing I-95N. It was one of the \$54-million Contract 8 project bridges, which connects I-95S to the new I-195E. Its seismic design is based on a 2,500-year return period earthquake.

**RIDOT, I-195 Relocation Project, Ramp Point Street Bridge, Providence, RI:** Principal Engineer for bridge design from start to finish. Responsible for sub-consultant coordination, bridge design and detail supervision, scheduling and budget management, drafting, engineering estimates and job-specific specifications, addenda, and assisting client's project bidding. Responsible for bridge static and seismic loading analysis, I girder design, steel detail design, field splice and shear studs design, cross frame and end support diaphragm design, and pier and foundation design. Part of the \$60-million Contract 11 of the I-195 relocation/improvement project, the bridge is a 3-span, 509-ft curved steel I girder continuous skewed bridge that crosses I-95 supported on spread footing.

**RIDOT, I-195 Relocation Project, Clifford Street Bridge, Providence, RI:** Principal Engineer for bridge design from start to finish. Responsible for sub-consultant coordination, bridge design and detail supervision, scheduling and budget management, drafting, engineering estimates and job specific specification, addenda, and assisting client's project bidding. Part of the \$60-million Contract 11 of the I-195 relocation/improvement project, the bridge is a 2-span, 360-ft steel I girder continuous skewed bridge that crosses I-95 supported on spread footing.

**RIDOT, I-195 Relocation Project, India Point Park Pedestrian Bridge, Providence, RI:** Principal Engineer for bridge design from 30% to completion, and supervision of bridge and retaining wall design. Responsible for pier column design, south abutment design, rib beam and deck design, concrete joint reinforcement detail in accordance with seismic requirements, and bridge construction coordination with client and contractor. This \$26-million project included a 2-span pre-cast box beam across I-195, a 2-span cast-in-place concrete rigid frame arch, and an 1,800-ft-long retaining wall. It is part of the East Providence India Point Park reconstruction, with maximum 17-ft soil, trees and landscape structures on the bridge.

**RIDOT, I-195 Relocation Project, Ramp WES Bridge, Providence, RI:** Principal Engineer for bridge design from 30% to finish, supervision of bridge design, and coordination of highway, geotech and architectural design, and design schedule. Responsible for final bridge alignment and span recommendation to client, bridge seismic and static load analysis and modeling, girder section, field splice and shear studs design, concrete pier and H-pile foundation design, supervision of drafting, estimates and construction specification. As one of the \$64-million Contract 6 bridges, Ramp WES is a 465-ft, 2-span curved steel box girder supported on H-pile foundation. This bridge connects the new I-195W to local traffic. Its seismic design is based on a 2,500-year return period earthquake.



**Yihui Peter Wu, PhD, PE**  
*Design Manager*

**RIDOT, Route 6 Bridges and Seismic Retrofit Project, Providence, RI:** Senior Engineer responsible for seismic analysis and isolation design review, isolation bearing, shop drawing review and approval, isolation bearing prototype test and quality control test approval, bearing installation and final inspection, and shop drawing review/approval. These bridges were part of the \$10.6-million Route 6 seismic retrofit project; isolation bearing was selected to reduce the cost to retrofit the pier and foundation under seismic loading.

**Massachusetts Department of Transportation (MassDOT) District 1, Route 8 over the B&M Railroad and Hoosic River (Hadley Overpass) Bridge Rehabilitation, North Adams, MA:** Project Manager and Principal Structural Engineer responsible for the project construction-phase service management, contract amendment, MOD and invoices, construction coordination, shop drawing approval and RFI response, contract budget, schedule and quality assurance and control. This \$25-million project includes replacing most of the 14 span steel girder and pier repair, truss span rehabilitation. The bridge is situated in the downtown area of North Adams; traffic is open during the construction, and staging construction is anticipated for this project.

**MassDOT District 4, Route 128/I-95 Add-a-Lane Project Bridge Replacement, Dedham/Needham/Westwood, MA:** As Project Structural Manager and Engineer-of-Record for Bridge Contract Bid Documents, responsibilities include managing design of 7 permanent and 2 temporary bridges, schedule, budget, and quality assurance and control. This \$100-million project includes about 3.6 miles of roadway realignment and widening; 7 bridges replaced for widening including Route 128S over the Charles River Bridge; Route 128N over the Charles River Bridge; Route 128S over Route 135 Bridge; Route 128N over Route 135 Bridge; Route 109 over Route 128N and 128S Bridges, and Great Plain Avenue over Route 128 Bridge. Two temporary bridges were designed for construction stage: Route 128S and Route 128N over Route 135 Temporary Steel Panel Bridge.

**MassDOT, District 4, Route 2/Crosby's Corner, Concord/Lincoln, MA:** Principal Structural Engineer, Responsible for quality control of bridge and retaining wall design, structural scope of work, budget, and schedule. This project provided oversight of complete final design phase engineering services to MassHighway for the Route 2 Crosby's Corner Project from Bedford Road in Lincoln to Sandy Pond Road in Concord. Project includes full engineering services from preliminary design to development of contract documents of 2 miles of highway and a steel box girder bridge.

**Massachusetts Highway Department, Calvin Coolidge Bridge Seismic Retrofit:** Senior Engineer responsible for seismic analysis review, isolation bearing design review, isolation bearing prototype test and quality control test approval, and shop drawing approval. The Calvin Coolidge Bridge is an historic steel truss bridge; application of lead core isolation bearing reduced the retrofit cost compared to the pier and foundation method of modification.

**Massachusetts Bay Transportation Authority (MBTA), Bridge Inspection and Rating Program, Systemwide Massachusetts:** Principal Structural Engineer responsible for quality control and supervision of the inspection and rating of 55 bridges and structures. Special assignment included emergency inspections and evaluation of deteriorated or damaged structures.

**MBTA, 7 Bridge Replacement/Rehabilitation Projects, Boston, MA:** Principal Structural Engineer responsible for contract scope of work, quality control of bridge replacement design from type study to finish, bridge construction phase services, and design supervision and coordination. The 7 bridges were situated in Greater Boston, and rail traffic was maintained during bridge construction. Staging constructions were used for the following bridge replacements or rehabilitations:



# David B. Freeman, PE

*Design QA/QC Manager*

**MAGUIRE GROUP INC.  
PRINCIPAL ENGINEER**

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**PROFESSIONAL PROFILE**

Mr. Freeman serves as Project Manager and Principal Engineer for Transportation Projects and has a variety of experience including both urban and rural roadway design, traffic analysis and impact studies, environmental studies, and utility design and coordination.

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**TECHNICAL EXPERTISE**

Transportation engineering  
Urban and rural roadway design  
Project management  
Highway design  
Environmental impact studies  
Traffic analysis  
Utilities design/coordination  
River relocation  
Roadway drainage systems

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**YEARS OF EXPERIENCE**

Maguire: Since 1985  
Total: Since 1979

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**EDUCATION**

BS/1979/Civil Engineering/Lehigh  
University

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**PROFESSIONAL  
REGISTRATIONS**

FL 67259, MA 39893, NJ GE29476, NY  
631801, RI 4870

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**PROFESSIONAL  
ASSOCIATIONS**

American Society of Civil Engineers  
Institute of  
Transportation Engineers

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**AWARDS**

RI Society of Professional Engineers  
Engineer of the Year – 2005

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**REPRESENTATIVE PROJECTS**

**Design and Oversight, Iway Relocation of Interstate 195, RIDOT, Providence, RI:** As Senior Project Manager, was responsible for supervising the preparation of the Final Design for this \$450-million project that will run through 2012. Supervision involved overseeing the efforts of a dozen subconsultants and over 30 Maguire staff. The project includes relocating almost two miles of urban interstate including the I-195/I-95 interchange through Providence. There are 16 bridges, the largest of which is a 900-foot-long, 10-lane-wide crossing of the Providence River. The construction sequence calls for 15 construction contracts ranging in value from \$1 million to \$85 million and an additional five demolition contracts. Final Design included a complete scope of engineering efforts on the part of Maguire Group and subconsultants that needed to be managed to bring the project to fruition. They include:

- • bridge design
- • city street design
- • traffic engineering
- • advertising support
- • construction services
- • geotechnical engineering/geotechnical exploration
- • marine engineering
- • utility design & coordination
- • survey
- • landscape design
- • architecture
- • interstate highway and interchange design
- • construction document preparation
- • environmental permit preparation (ACOE, CRMC, RIDEM)
- • contaminated soil management
- • right-of-way acquisition documents
- • development of construction sequence
- • construction time determination and monitoring
- • coordination with city, state, and federal agencies

**I-95 Ramps Project, Providence, RI:** Mr. Freeman served as Project Manager as a subconsultant on a design team to improve access into Downtown Providence to support major retail development. Design responsibilities included reconstruction of a freeway terminal at the interface with arterial streets, accommodation of significant pedestrian movements, interconnected signal design, and preparation of documentation to gain approval from the Capital Center Commission, a quasi-public design review committee.



## **David B. Freeman, PE** *Design QA/QC Manager*

**Interim Resurfacing and Bridge Repairs, I-195, Providence, RI:** Mr. Freeman was Project Manager for this contract which included preparation of contract plans and documents to resurface I-195 through downtown Providence, repair bridge decks, and provide supplemental shoring systems for deteriorated substructure elements under the bridges. The effort included evaluating alternative sequences of construction to select the best method and time of day to minimize impacts on interstate traffic.

**Traffic Analysis and Road Improvement Design, Routes 146 and 116 Interchange, RIDOT, Lincoln, RI:** Route 116 is a major corridor in Lincoln, Cumberland and part of Smithfield, RI; Route 146 runs from Providence north to the Massachusetts border. The Routes 116-146 interchange was the first cloverleaf interchange to be constructed in RI, and major reconstruction began in 2005. As Project Manager, Mr. Freeman studied reconstruction alternatives that included the analysis and prediction of future traffic conditions for the various alternatives. Also prepared the preliminary design for improvements to 2.5 miles of Route 146 and two miles of Route 116.

**Environmental Impact Statement – Improvements to I-195, Providence, RI:** Responsible for preparing an Environmental Impact Statement (EIS) for the relocation of I-195 through downtown Providence. The EIS investigated various alternatives for improvements to a major urban interstate and an interchange with I-95, which included development of future traffic patterns, analysis of levels of service, change of access request, and a determination of the feasibility of the alternatives in terms of geometric adequacy and estimated construction costs. In addition, it fully evaluated the environmental and social impacts, urban design issues, and 4f/6f and Section 106 issues.

**River Relocation and Street Reconstruction, Memorial Boulevard Extension, Providence, RI:** As a Project Engineer, Mr. Freeman's responsibilities included preparation of street reconstruction plans and specifications for the third, fourth and fifth contracts of a six-contract series to relocate rivers and construct a major boulevard and waterfront pedestrian esplanade through Providence. His duties included urban street design, coordinating with the bridge design and utility relocation efforts, detailing maintenance and protection of traffic plans, and support during construction.

**Interim Deck Repairs, I-195 and Providence River Bridge, Providence, RI:** Mr. Freeman served as Project Manager for this project. In addition, his design responsibilities included preparing a four-phase traffic control plan to help keep traffic moving during deck rehabilitation on I-195 and preparing environmental permit applications for approval of hydro-demolition on the deck. He was responsible for expediting preparation of contract documents, assisting with a comprehensive public relations campaign, coordinating bridge design, and coordinating with public officials.

**Route 55 Freeway Drainage Project, Southern New Jersey:** Served as an Assistant Project Engineer in the preparation of plans, specifications and estimates for several sections of Route 55. He was responsible for layout and design of drainage systems, including detention basins, and municipal water and sanitary sewer relocation designs. He prepared NJ Department of Environmental Protection (DEP) permit applications. In addition, he developed an algorithm for computer analysis of hydraulic grade lines which he applied to the drainage design for this project.

THIS LICENSE IS ISSUED IN ACCORDANCE WITH THE REQUIREMENTS OF TITLE 5, CHAPTER 8, PUBLIC LAWS, STATE OF RHODE ISLAND

2476599  
REBLUE

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF BUSINESS REGULATION • DIVISION OF DESIGN PROFESSIONALS  
BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS  
THE REGISTRANT HAS MET THE REQUIREMENTS OF THE LAW AND HAS BEEN GRANTED THIS CERTIFICATE OF REGISTRATION AS A PROFESSIONAL ENGINEER

REGISTRANT (VALID WHEN STAMPED) REGISTRATION NUMBER  
**DAVID B. FREEMAN 4870**  
**MAGUIRE GROUP, INC.**  
**225 CHAPMAN STREET**  
**PROVIDENCE, RI 02905**

EXP. DATE  
**06/30/2011**

SIGNATURE (NOT VALID UNLESS SIGNED)

**IMPORTANT**

If this is lost or destroyed, notify in writing, Board of Registration for Professional Engineers, 1511 Pontiac Ave., Bldg. 68-2, Cranston, RI 02920. If name or address shown hereon is changed, notify your Board in writing, of your correct name or address to insure proper mailing of next Renewal Application. Always refer to your registration number.

Registration is subject to the provisions of the General Laws as amended. It is a personal privilege and must not be loaned or assigned to any other person. Keep this license on your person or posted as required by law.

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# William G. Young, PE

## Civil Engineer

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**MAGUIRE GROUP INC.**  
**CHIEF CIVIL ENGINEER**

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### TECHNICAL EXPERTISE

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Design/Build Project Delivery  
Value Engineering  
Site assessments and design  
Hydraulic analysis/modeling  
Roadway design  
Contract documents  
Topographic surveys  
Traffic impact analysis  
Planning  
Permitting  
CAD/D expertise

### YEARS OF EXPERIENCE

Maguire: 1998 – 2007  
2010-Present  
Contech CPI: 2007-2010

### EDUCATION

BS/1998/Civil Engineering  
Associate of Science/1994  
Pending/Certificate in Engineering  
Management

### TECHNICAL CONTINUING EDUCATION

Water Surface Profiling and Floodplain  
Analysis  
(HEC-RAS Methodology)  
Quarterly Contech University 2 day  
off-site workshops

### PROFESSIONAL REGISTRATIONS

Professional Engineer - Civil, RI 7867

### PROFESSIONAL ASSOCIATIONS

Past President, RI Section of the  
American Society of Civil Engineers

### PUBLICATIONS

"Economic Optimization of Sustainable  
Subsurface Infiltration  
Systems" CE NEWS, Professional  
Development Series (November  
2009): Professional Development  
Advertising - Section 1 PDH - 6. Print.

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### PROFESSIONAL PROFILE

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Mr. Young is a registered professional engineer with over 12 years of experience in civil/site and transportation engineering. He has been the core engineer on a wide range of federal, state, municipal, and private sector projects. Mr. Young is known throughout the industry as a provider of highly sustainable, value engineered, low-impact development solutions due to his extensive background in not only consultative engineering, but also engineered solutions sales. His responsibilities have included site assessment, horizontal and vertical site design, roadway and intersection design, hydraulic and hydrologic analysis and design, traffic impact analysis and design, plan/contract document preparation and quality control, permit preparation/execution, quality control and execution of electronic deliverables.

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### REPRESENTATIVE PROJECTS

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**Bridge Replacement Br. No. H-06-003=H-07-003 (Concrete), Broadway/Winter Streets over the Indian Head River, Hanover-Hanson, MA (MassDOT Accelerated Bridge Program):** Responsible for identifying, preparing, and presenting a successful value engineering proposal on behalf the winning contractor, AA Will Corporation. The solution contained within the original contract documents was for an "in kind" replacement of a cast-in-place 21' span reinforced concrete arch on a 19 degree skew to the roadway. Due to the dewatering complexities associated with pouring new foundations within the river all the way to bedrock as well as required wetland replication for compensation, AA Will Corp. preferred to do all foundation work outside of the existing channel and avoid wetland impacts/replication altogether. Mr. Young presented a 54' span (no skew) Bebo® complete pre-cast bridge system with TL-4 crash rated headwalls and wingwalls. The existing hydraulic opening beyond the 10 year storm event was purposely matched by leaving the historic granite abutments in place. The solution was presented on behalf of AA Will at the MassDOT project kick-off meeting and was well received by all project stakeholders. The proposal included tearing down the existing arch to the spring line and then simply repairing the existing granite abutments above the waterline. This solution reduced several environmental as well as socioeconomic impacts by staying out of the wetlands as well as reducing the construction time from 2-1/2 years to 7 months while completely eliminating phased construction which included a long term signal controlling alternating one-way traffic. The original agreed upon contract amount was \$1,392,000, and AA Will Corporation is now providing a contract credit to MassDOT for \$303,000, or an overall savings to the taxpayers of 12%.

**Ocean Avenue Seawall, Ocean Avenue/Harrison Avenue, Newport, RI (City of Newport):** Responsible for identifying and coordinating a winning design/build team as well as developing a completely new, custom/hybrid Re-Con®/Tensor® Geogrid and Armorflex® complete replacement solution for a failing section of seawall, circa 1930's. Of the nearly twenty teams who submitted proposals, the Contech/Gary K. Munkelt & Associates/Mello Construction Co. team was the only one to propose a complete replacement of the failing wall. Our proposed solution was still well within the total project budget and competitive with other teams who were only proposing a "cap and repair" solution.



## **William G. Young, PE** *Civil Engineer*

**Pawtucket/Mineral Spring Avenue ADA Compliance, East Providence/North Providence, RI (RIDOT):** Upgrade/addition of highway pavement stormwater collection systems, reconstruction of sidewalks, wheelchair ramps, and crosswalks. The project was to promote handicap accessibility as well as pedestrian safety along 1.7 miles of state highway. The project also included the re-alignment of the Pawtucket Ave./Bullock's Point Ave. Intersection to address safety concerns. U.S. Route 6 Improvements, Johnston/Providence, RI (RIDOT): Project Engineer responsible for electronic contract document compilation for improvements to 3.6 miles of this major East/West four-lane highway. The reconstruction of six bridges was also included within the contract.

**Sail Newport Boat Ramp Renovation, Fort Adams State Park, Newport RI (GridTech/Sail Newport):** Project Consultant responsible for developing a successful value engineering proposal for the rehabilitation of a World War II cast-in-place concrete boat ramp with our distributor, GridTech Inc. A pre-cast concrete slab system was specified for the project which would have required significant environmental impacts via the demolition and removal of the existing ramp. In conjunction with GridTech, the owner (Sail Newport), the Rhode Island Coastal Resources Management Council (CRMC), and the engineer of record for the original design, a hybrid system of Tensar® BX Geogrid and ArmorLoc® 5011 articulating concrete blocks which overlaid and protected the existing ramp from future hydraulic undermining was developed and ultimately installed. It is estimated that this successful value engineering design saved the owner approximately 50% over the original design. The ArmorLoc® system also allows for launching from the side of the ramp as well due to its smooth transition below grade. Sail Newport hosts several dinghy regattas each year which draw well in excess of 100 boats which all need to be launched in a short timeframe, and the ability to launch off the side of the ramp greatly increases their capacity.

**F.M. Global New Corporate Headquarters Retaining Walls, Johnston, RI (Dimeo Construction Co.):** Project Consultant responsible for developing and coordinating an aesthetically pleasing gravity retaining wall system to be used throughout the heavily constrained site. In early 2008, Mr. Young was approached by the team of VHB Consulting Engineers and Vision III Architects to assist them in designing an alternative to cast-in-place retaining walls that had the look of natural granite while not using any geogrid due to multiple retaining wall conflicts. In conjunction with Gary K. Munkelt & Associates, a modified Re-Con® wet-cast modular block retaining wall system was recommended and ultimately constructed. Six walls totaling over 30,000 square feet were built by the site sub-contractor (Digregorio Corp.) which ultimately saved tremendous time and money when compared to what it would have been required with a stone veneered cast-in-place wall system.

**Pocasset River Relocation/Stream Bank Stabilization and Flood Hazard Mitigation, Johnston, RI (Hobbs Brook Management):** Principal Engineer responsible for the protection and future relocation of a stream bank along the Pocasset River. While the project was being completed under an agreement with a private corporation, it was also being done in conjunction with the USDA's Natural Resources Conservation Service's (NRCS) "Pocasset River Watershed Flood Plain Management Study". This coordinated approach was intended to solve a specific erosion problem for the client while improving downstream water quality and decreasing the risk of flood damage to the adjacent building. In addition to the work within the river, the large adjacent office building was slated to be flood proofed beyond the 500 year storm event by use of floodwalls and a reconfigured storm drainage system. At the time the study was undertaken, the building was not listed within the 100 year flood plain, but this will change once the results of the above referenced study are incorporated into the Federal Emergency Management Agency's Flood Insurance Rate Maps. By flood-proofing the building prior to that change, the client is able to construct a flood wall without providing additional on-site floodplain storage. Responsibilities included the inventorying, modification, and coordination of all HEC-RAS hydraulic model analyses with the NRCS (Joel Schmidt), as well as the Rhode Island Department of Environmental Management (RIDEM). Additional cross sections were ordered and the model was calibrated to rainfall data and physical observations related to the October 15, 2005, 100 year flood event. Responsibilities also included the realignment design of the channel immediately upstream of the Samson AMI Pond to eliminate further erosion of the eastern river embankment.

THIS LICENSE IS ISSUED IN ACCORDANCE WITH THE REQUIREMENTS OF TITLE 5, CHAPTER 8, PUBLIC LAWS, STATE OF RHODE ISLAND

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF BUSINESS REGULATION • DIVISION OF DESIGN PROFESSIONALS  
BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS  
THE REGISTRANT HAS MET THE REQUIREMENTS OF THE LAW AND  
HAS BEEN GRANTED THIS CERTIFICATE OF REGISTRATION AS A  
PROFESSIONAL ENGINEER **CIVIL**

REGISTRANT (VALID WHEN STAMPED)

**WILLIAM G. YOUNG**  
**34 DAHLIA ST**  
**WARWICK, RI 02888**

REGISTRATION  
NUMBER

**7867**

EXP. DATE

**06/30/2011**

SIGNATURE (NOT VALID UNLESS SIGNED)

### IMPORTANT

If this is lost or destroyed, notify in writing, **Board of Registration for Professional Engineers, 1511 Pontiac Ave., Bldg. 68-2, Cranston, RI 02920**. If name or address shown hereon is changed, notify your Board in writing, of your correct name or address to insure proper mailing of next Renewal Application. Always refer to your registration number.

Registration is subject to the provisions of the General Laws as amended. It is a personal privilege and must not be loaned or assigned to any other person. Keep this license on your person or posted as required by law.

THIS LICENSE IS ISSUED IN ACCORDANCE  
WITH THE REQUIREMENTS OF TITLE 5,  
CHAPTER 8, PUBLIC LAWS, STATE OF  
RHODE ISLAND

STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF BUSINESS REGULATION • DIVISION OF DESIGN PROFESSIONALS  
BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS  
THE REGISTRANT HAS MET THE REQUIREMENTS OF THE LAW AND  
HAS BEEN GRANTED THIS CERTIFICATE OF REGISTRATION AS A  
**PROFESSIONAL ENGINEER STRUCTURAL**

REGISTRANT

(VALID WHEN STAMPED)

REGISTRATION  
NUMBER

**MATTHEW J. WEIDELE**  
**31 COLESONIAN DR**  
**WARWICK, RI 02888-1505**

**7861**

EXP. DATE

**06/30/2011**

  
SIGNATURE (NOT VALID UNLESS SIGNED)

## IMPORTANT

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STATE OF RHODE ISLAND AND PROVIDENCE PLANTATIONS  
DEPARTMENT OF BUSINESS REGULATION - DIVISION OF DESIGN PROFESSIONALS  
BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS  
THE REGISTRANT HAS MET THE REQUIREMENTS OF THE LAW AND  
HAS BEEN GRANTED THIS CERTIFICATE OF REGISTRATION AS A  
PROFESSIONAL ENGINEER  
CIVIL

REGISTRANT	(VALID WHEN STAMPED)	REGISTRATION NUMBER
SKYLER D. MILLS		6896
35 JOHN KESSON LANE		
MIDDLETOWN, RI 02842		EXP. DATE
Skyler D. Mills		06/30/2013
SIGNATURE (NOT VALID UNLESS SIGNED)		

**IMPORTANT**

If this is lost or destroyed, notify in writing, **Board of Registration for Professional Engineers, 1511 Pontiac Ave., Bldg. 68-2, Cranston, RI 02920**. If name or address shown hereon is changed, notify your Board **in writing**, of your correct name or address to insure proper mailing of next Renewal Notification. **Always refer to your registration number.**

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# Joseph Cardello, III, PE

## Lead Roadway Engineer

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MAGUIRE GROUP INC.  
CIVIL ENGINEER

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### TECHNICAL EXPERTISE

Horizontal and Vertical Geometric Design of small to Complex Highway Design and Reconstruction Projects  
Project coordination for multi-disciplined transportation projects.  
Preparation and compilation of Plans, Specifications, Quantities and Project Estimates  
Maintenance and Protection of Traffic Control Plans for local streets to Major Interstate Highways  
Existing and Proposed Drainage & Utility Design & Analysis  
Project permitting including the implementation of Best Management Practices

### YEARS OF EXPERIENCE

Maguire: Since 1994  
Total: Since 1986

### EDUCATION

BS/1986/Civil Engineering  
University of Rhode Island

### PROFESSIONAL REGISTRATIONS

Professional Engineer  
MA 37316, RI 6020

### PROFESSIONAL ASSOCIATIONS

American society of Civil Engineers  
Rhode Island society of Professional Engineers  
Providence Engineering Society

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### PROFESSIONAL PROFILE

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Mr. Cardello's experience extends into several areas of highway design and reconstruction projects. Mr. Cardello has experience in developing conceptual to final design plans and specifications for minor road reconstruction projects to complex interstate relocation projects. His tasks include establishing horizontal and vertical geometry, identifying constructability issues, maintenance and protection of traffic plans, open and closed drainage analysis and design, water quality designs and Best Management Practices, and grading plans. He is also experienced in preparing contract documents including project specifications, distribution of quantities and construction cost estimates.

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### REPRESENTATIVE PROJECTS:

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**Improvements to Interstate Route 195 (Project wide):** Lead Highway Engineer for the \$600-million project, to be constructed via 15 contracts over 12 years. Responsible for the overall design of the horizontal and vertical alignments of I-95, I-195, major interstate ramps, on and off ramps to local streets, drainage design, utility design, relocations and coordination, compilation of plans, specifications, quantities and estimates for individual contracts, project design and construction sequencing and schedule development. To date, five contracts are complete and five are in various stages of construction. As a result of the construction to date, one new interstate to interstate ramp is open to traffic and three others are in construction with scheduled openings over the next 2 years. Existing ramps have been closed and new ramps have been opened. The design, relocation and construction of over 10,000 feet of electrical duct, 3,600 feet of overhead high-voltage electrical lines, 600 feet of telephone duct, 1,000 feet of overhead fire alarm cable, 5,000 feet of water main, 3,000 feet of high-pressure gas main, 2,000 feet of low-pressure gas main, 500 feet of communications cable, and 1,400 feet of 54" combined sewer lines have coordinated with the project. The relocation and removal of a 136,000-barrel oil tank (owned by PG&E Generating, formerly Narragansett Electric) and the associated connections to the power plant were coordinated with the I-195 project in advance of the earliest contracts. Today, a new oil tank located south of the lway provides a second source of fuel for the power plant to the north. Additional improvements and relocations are included with the most recent and future contracts.

**Improvements to Interstate Route 195, Contract 1:** South Water Street from Crawford Street Bridge to James Street. Design and relocation / realignment of South Water Street, including drainage design and utility relocations. The contract also included the design and construction of new riverwalls and boardwalk along the east bank of the Providence River. Completed June 2002.

**Improvements to Interstate Route 195 Contract 2:** Storm Drain Retrofit Demonstration Project. Design and implementation Best Management Practices (BMP) for existing storm drain systems. The projected consisted of combined sewer separation, subsurface engineered stormwater treatment systems and a three staged water quality pond. Completed April 2000.



**Joseph Cardello, III, PE**  
*Lead Roadway Engineer*

**Camp Edwards, Gymnasium Renovation, Massachusetts Military Reservation:** Project Architect for a feasibility study for the 26,000-SF, 1940's era gymnasium. Proposed renovation includes exterior restoration, and a new addition for locker and toilet rooms and new MEP systems throughout. Phase II consisted of completing construction documents for the exterior restoration which included

**Improvements to Interstate Route 195, Contract 5:** Advanced Embankment Construction, Test Pile Program & Building Demolition: Planned as an advanced contract, work in this contract included placing approximately 175,000 cy of earth to a height of 30' above existing grade to consolidate organic soils located within the proposed interchange for I-195 and I-95 on the west side of the Providence River. The south face of the embankment be armored with 2 – 4 ton boulders and act as a new hurricane barrier for the City of Providence. Completed November 2004.

**Waterfront Drive, East Providence, RI:** Analysis of existing drainage conditions along the project corridor. Preliminary design of the proposed drainage for the project.

**Reconstruction of Mendon Road, Route 122, Cumberland RI:** Extensive utility relocations, including gas, water, telephone, electric, and sanitary sewers were required on this project to accommodate the proposed drainage design. Distribution of quantities and specifications.

**Reconstruction of Route 146, Lincoln, RI:** Horizontal and vertical design for the reconstruction of this primary route. Additional work on this project included drainage and pavement markings. Prepared distribution of quantities and specifications.

**Route 138, Jamestown Bridge Approach, North Kingstown, RI:** Modifications to the existing/proposed drainage design being constructed to include a water quality design (B.M.P.). Prepared distribution of quantities and specifications.

**Reconstruction of Route 44, Smithfield, RI:** Review of the impacts to wetlands and the 100-year flood plain by the proposed highway improvements. Analysis consisted of providing areas and computations for wetland replacement and flood plain compensation.





# James W. Coogan, PE

## Traffic Engineer

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**MAGUIRE GROUP INC.**  
**PRINCIPAL TRAFFIC ENGINEER**

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**TECHNICAL EXPERTISE**

Roadway and Traffic Design and  
Planning  
Conceptual and Final Designs  
Drainage Design  
Community Relations

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**YEARS OF EXPERIENCE**

Maguire: Since 1979  
Total: Since 1978

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**EDUCATION**

BS/1977/Civil Engineering/  
Transportation  
Rensselaer Polytechnic Institute

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**PROFESSIONAL  
REGISTRATIONS**

Professional Engineer  
FL 70256  
MA 48476  
RI 4478

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**PROFESSIONAL  
ASSOCIATIONS**

Providence Engineering Society  
President, Board of Directors

American Society of Civil Engineers

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**PROFESSIONAL PROFILE**

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Mr. Coogan serves as Transportation Project Engineer and has participated in a variety of transportation projects. His primary project responsibilities are design, plans, specifications, estimates, and coordination with clients, agencies, and local communities.

He has extensive roadway design experience, including conceptual design reports and preparation of final design documents for 3R and 4R improvement projects, horizontal and vertical alignment, drainage design, pavement design, and right-of-way (ROW) condemnation documents.

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**REPRESENTATIVE PROJECTS**

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**Statewide ADA Compliance Program, Rhode Island Department of Transportation (RIDOT), 15 Rhode Island Communities:** Project Manager and Lead Engineer on a two-phase project for the Rhode Island Department of Transportation. Phase 1 of the program involves a complete inventory of accessibility deficiencies on RIDOT roadways within the assigned communities, and developing a database of noncompliant features and estimated costs for remediation. Under Phase 2, Maguire Group assists RIDOT in identifying high priority facilities, and is developing design plans and bid documents for construction on these roadways.

**Reconstruction of U.S. Route 6, Johnston/Providence, RI:** Project Manager and Lead Design Engineer for improvements to this 6-lane freeway facility that had been begun by another consultant. After assisting the Department in revising the entire project concept that had been developed by another consultant, construction documents were developed to accomplish a new menu of recommended improvements. Three separate construction contracts were prepared, permitting road improvements to this four-mile freeway in two separate segments, followed by bridge rehabilitation to seven (7) bridges within the project. The construction of these three contracts, totaling approximately \$16 million, was accomplished over a five-year period, concluding in 2005.

**Traffic Calming Improvements to Fox Point, Providence, RI:** Project Manager and Lead Design Engineer for traffic calming measures and streetscape improvements associated with the rehabilitation of the intersection of Wickenden Street and Governor Street. This project required extensive coordination with the City's Planning and Development Department, the Department of Public Works, and the Parks Department. In addition, final design alterations were made to accommodate the needs of a local elementary school abutting the project to the south, and the ongoing design of improvements to East Street, which were driven by the Pedestrian Overpass Bridge portion of the I-195 Relocation Project increasing pedestrian safety, rehabilitating and connecting sidewalk sections, and signal improvements. Construction documents were prepared, and the project was built in 2002.



## **James W. Coogan, PE** *Traffic Engineer*

**Signal Optimization Project, Rhode Island Department of Transportation (RIDOT), Northern RI Communities:** Project Manager for the intersection analysis, conceptual design improvements, and final design of signal design modifications and construction document preparation for twenty-six intersections in six communities in Northern Rhode Island.

**Route 2 Access Management Plan, East Greenwich, RI:** Project Manager and Lead Traffic Engineer for this project which evaluated the potential impacts of development on the character of the Route 2 corridor between Division Street and Frenchtown Road. This study developed and evaluated access management techniques appropriate to ensure safe passage for vehicles, pedestrians, bicyclists and transit along the corridor as land uses continued to intensify. The result was a master document detailing future site-specific access management strategies for the Town to apply as conditions for development. Participated in the preparation/presentation of public workshops and document development, and ultimately presented the project at an Access Management Workshop co-sponsored by the Rhode Island Division of Planning and the FHWA.

**Atlantic Beach District Master Plan, Town of Middletown, RI:** Lead Traffic Engineer for the study that looked to develop a master plan of concepts and strategies that would make the Atlantic Beach District more walkable and cohesive neighborhood that serves the local residents and businesses but also the many tourists that visit Aquidneck Island (primarily Newport) in the summer. Topics addressed were improving the pedestrian environment, addressing vehicle/bicycle conflicts, addressing the need for parking, solving traffic circulation problems, and creating a more attractive area. Public workshops were held to collect feedback and to discuss possible concepts and solutions.

**I3R Improvement Projects throughout Rhode Island, Rhode Island Department of Transportation:** Project Manager and Lead Design Engineer for fourteen design locations throughout the State, with each location ranging from 0.3 to 5.2 miles in length. Prepared Conceptual Design Reports and Final Design / Construction Documents for 14 different road improvement projects, constructed under 18 contracts addressing road rehabilitation, sidewalk work, signalization and bridge rehabilitation. Locations included: Interstate Route 95 Service Roads in Providence and Pawtucket, R.I.; as well as Waterman Avenue, Taunton Avenue, Bullocks Point Avenue, and Highland Avenue, East Providence; Point Street, Eddy Street, and Atwells Avenue, Providence; Mayfield Avenue and Cranston Street, Cranston.

**State Highway Resurfacing/Safety Improvements, Rhode Island Department of Transportation (RIDOT), from Connecticut State Line to Exit 7 (New London Turnpike):** Project Manager for riding surface improvements and safety improvements to a 21-mile segment of Interstate 95 and adjoining ramps in the southern half of Rhode Island. This segment is a predominantly rural, 4-lane divided interstate highway on rolling terrain. As part of this contract, extensive investigations of pavement structure conditions, drainage facilities and wetland impacts were conducted. Design improvements implemented in two separate construction contracts, were developed to rehabilitate this deteriorating facility, resulting in a smooth-riding, safe, long lasting facility, with minimal impact to the many wetlands adjacent to the highway. Maintenance of traffic was a paramount concern, and careful attention to traffic control schemes, lane closures, and work hours required detailed analysis and specifications.

**High Hazard Location Program, RIDOT, Various Rhode Island Roadways:** Project Manager for safety improvements including highway widening and traffic signal improvements, as well as sidewalk, road realignment and drainage improvements to various locations in Rhode Island including Pitman and Gano Street, and North Main Street at Branch Avenue and at Olney Avenue, Providence, and West Main Road at Stringham Road, Portsmouth.



# Brian Guzas, PE

## Structural Engineer

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**MAGUIRE GROUP INC.**  
**PRINCIPAL ENGINEER**

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### TECHNICAL EXPERTISE

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Bridge load rating analyses  
Follow-up inspections  
Quantity calculations  
Cost estimates,

### YEARS OF EXPERIENCE

Maguire: Since 2010  
Total: Since 2000

### EDUCATION

Master of Civil Engineering/2005/The  
John Hopkins  
Bachelor of Science in Civil  
Engineering (Summa Cum Laude)/  
2000/Lafayette College

### PROFESSIONAL REGISTRATIONS

MD, NY, PA, MA and VT

National Council of Examiners for  
Engineering & Surveying (NCEES)  
Council Record/November, 2005 –  
Present

### PROFESSIONAL ASSOCIATIONS

Member, American Society of Civil  
Engineers (ASCE)

### SPECIALIZED TRAINING

Safety Inspection of In-Service Bridges  
Training Course/USDOT, FHWA, NHI

Campbell Scientific “CR9000 Training  
Course”

OSHA 29 CFR 1910.146 “Confined  
Space Entry Training”

SEPTA Track Safety Course

ALGOR “Introduction to Mechanical  
Event Simulation” Training Course

American Society of Engineering  
Companies (ACEC): “Partnering for  
Success” Seminar

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### PROFESSIONAL PROFILE

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Mr. Guzas has over 10 years experience in structural engineering. He has performed bridge load rating analyses, follow-up inspections, quantity calculations, cost estimates, and detail checking of engineering computations and contract plans. His construction phase services including shop drawing review, field meetings and RFI review/response. Prepared alternatives analyses, design criteria, design reports and bid proposal documents for county/municipally managed federal/ state aid projects. Designed steel plate girder, steel rolled beam, and concrete segmental highway and railroad bridges. Designed retaining walls, sound barriers and retrofit/repairs for existing bridges.

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### REPRESENTATIVE PROJECTS

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**Vermont Agency of Transportation (VAOT), US Route 2 over Winooski River, Richmond, VT:** Project Engineer responsible for drafting design-build solicitation documents (Request for Qualifications, Request for Proposals) for rehabilitation and widening of this 350-foot long through truss bridge and associated approach roadway realignment and reconstruction. Worked closely with VAOT to ensure compliance with legislative authority and agency standards and policies as well as considerations for a successful design-build project based on prior experience of others.

**Maryland State Highway Administration, US Route 50 Bridge over the Wicomico River, Salisbury, MD:** Designed structural repair details for the superstructure of this single-leaf bascule span movable bridge carrying MD 50 over the Wicomico River. Repairs included replacement of cross bracing members and strengthening of the bascule girders in deteriorated areas.

**New York State Department of Transportation (NYSDOT), Route 17 (I-86) Bypass Final Design, Parkville & Liberty, NY:** Project Engineer responsible for bridge design, detail checking, and coordination of staff for contract plan development. This project involves bridge replacement design for four mainline bridges for the upgrade of NYS Route 17 to interstate standards. The scope of work included design of four bridges spanning environmentally significant groundwater seeps and existing roadways. Structures designed included two composite single span steel plate girder bridges supported on cantilever abutments founded on spread footings, and substructures founded on stepped spread footings for two precast concrete 3-sided frame structures.

**Pennsylvania Department of Transportation, SR 3003 over Pine Creek, Lycoming County, PA:** This project involves final design and construction phase services for the rehabilitation of a historic 287-foot wrought iron, single-span lenticular truss bridge built in 1889 by the Berlin Iron Bridge Company. The rehabilitation includes replacement of the entire floor system, longitudinal girders, vertical hangers, low chord, pins and ornamental end posts, and selective rehabilitation of other existing components to remain as part of the final structure. Approximately 825 feet of approach roadway will be reconstructed, involving vertical and horizontal realignment, intersection reconstruction, and drainage improvements. Construction cost: Approximately \$5,500,000.



**Brian Guzas, PE**  
*Structural Engineer*

**Broome County Department of Public Works, New York, South Street Bridge over Halfway Brook, County Route 133 over Halfway Brook, Town of Barker, NY:** Project Engineer responsible for bridge evaluation and rehabilitation design. This project involves the study of rehabilitation and replacement alternatives for the existing 62', single-span, composite steel beam bridge. The scope of services for this Locally Administered Federal Aid Project includes bridge and approach roadway design; survey and mapping; geotechnical investigations; environmental studies; hydrologic and hydraulic analysis; design approval document preparation; acquisition of regulatory permits; public information meeting presentations; preparation of final plans, specifications and estimates; and construction inspection services. This project requires review and coordination with Broome County and the NYSDOT Region 9 Local Projects Unit. Project efforts are ongoing.

**Tioga County Department of Public Works, New York, Glen Mary Drive Bridge Rehabilitation, Glen Mary Drive over Catatonk Creek, Owego, NY:** This bridge (3-span [56'-69'-56'] continuous rolled steel beam bridge) required immediate rehabilitation due to the development of a full-depth penetration through the bridge deck and the potential for additional local failures. A fast-track project schedule, with a maximum of four months from project initiation to advertisement, was developed. Bridge inspection, field survey, coordination with environmental agencies, development of the design approval document, bridge design and contract plan development, including NYSDOT's review periods, were all completed in less than the allotted 4 months.

**Massachusetts Highway Department, Route 3A over Merrimack River, Tyngsborough, MA:** Bridge Engineer responsible for detail checking final design, load rating and seismic analysis calculations for this single 547-foot span steel through arch structure.

**Town of Griswold, Connecticut, Bitgood Road Bridge #4669 over Pachaug River Replacement, Griswold, CT:** Replacing an existing bridge with a new 61.5' clear span, prestressed, precast 24" deep box beam superstructure. The scope of services includes approximately 600 feet of roadway reconstruction, as well as minor alignment adjustments to correct deficient vertical grades and cross slopes and to increase the hydraulic opening. Work tasks included the development of bridge replacement alternatives for presentation to the Town, bridge and roadway plans, environmental permitting, approach roadway improvements, ROW plan development, hydraulic analysis, and preparation of bid documents.

**Tioga County Department of Public Works, New York, Southside Drive over Pumpelly Creek Bridge Replacement, Southside Drive Bridge over Pumpelly Creek, Owego, NY:** This project includes the replacement of an existing two-span, prestressed concrete box beam bridge, which had areas of serious deterioration and exposed prestressing strands. It was previously determined that replacement of the structure would be more cost-effective than rehabilitation. The scope of services for this project includes bridge and approach roadway design; survey and mapping; geotechnical investigations; environmental studies; design approval document preparation; acquisition of regulatory permits; public information meeting presentations; preparation of final plans, specifications and estimates; and construction inspection services. Construction funded through the American Reinvestment and Recovery Act (ARRA) of 2009. Construction Cost: \$1,224,000

**Ulster County Department of Bridges and Highways, New York - Main Street (CR 77) over Rondout Creek, Wawarsing, NY:** This Locally Administered Federal Aid project involved the replacement of a deteriorated historic single span steel truss bridge. The replacement bridge included a 49 meter long, two span, adjacent prestressed concrete box beam superstructure with a cast-in-place concrete abutment on piles, a cast-in-place concrete abutment on spread footings founded on rock, and a center pier on drilled shafts in rock. Approximately 200 meters of approach roadway improvements (including improvements to an intersection located adjacent to the bridge) were also required to raise the profile of the roadway near the bridge to satisfy hydraulic requirements. Construction funded through the American Reinvestment and Recovery Act (ARRA) of 2009.



# Martin J. Pierce, PE

## Lead Structural Design Engineer

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MAGUIRE GROUP INC.  
SENIOR BRIDGE ENGINEER

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### TECHNICAL EXPERTISE

Prestressed Precast Concrete Beams  
Structural Analysis/  
Finite Element Software  
Steel/Timber Design  
Structural/Bridge Inspections

### YEARS OF EXPERIENCE

Maguire: Since 2003  
Total: Since 1995

### EDUCATION

BS/1996/Structural Engineering  
University of Massachusetts  
MS/2003/Structural Engineering  
University of Rhode Island

### TRAINING

OSHA-10 Hour Construction  
Safety & Health  
OSHA-Permit Required  
Confined Space Entry  
AMTRAK – On-Track  
Safety Training for Contractors

### PROFESSIONAL REGISTRATIONS

Professional Engineer: RI 7446

### PROFESSIONAL ASSOCIATIONS

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### PROFESSIONAL PROFILE

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Mr. Pierce's responsibilities included the structural analysis and design of various bridge components in addition to the preparation of contract plans, specifications and estimates. As a senior bridge engineer, his responsibilities also include the structural field inspection and documentation of existing bridge conditions. His design experience includes simple and continuous span highway/pedestrian bridges, steel framed buildings and timber framed structures.

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### REPRESENTATIVE PROJECTS

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**I-195 Relocation, Providence, RI:** Responsibilities included the design and plan development of a two-span pre-cast box beam pedestrian bridge spanning the interstate. Along with the design of five cantilever retaining walls adjacent to interstate. Also assisted in the design and detailing of a 2 span concrete rigid frame associated with the above box beam bridge.

**Bellville Pond Dam Reconstruction & Pedestrian Bridge Replacement, N. Kingstown, RI:** Performed the design and plan development of a new timber pedestrian bridge as part of a dam reconstruction project located along the Ryan Park nature trail around Bellville Pond. The structure consisted of new timber stringers, deck and railing supported on existing concrete abutments. The project also included the replacement of the existing spillway with a new cast-in-place structure adjacent to an existing fish ladder.

**I-95 Service Roads, Bridges 547 & 548, Pawtucket, RI:** Responsibilities included the design and plan development for the structural rehabilitation of the existing bridges. The improvements included incorporating seismic isolation bearings in the design of the three intermediate pier replacements for each structure.

**Van Zandt Avenue Bridge, Newport, RI:** Performed field inspection of a three span monolithic concrete tee beam bridge spanning the Old Colony Railroad Line. Responsibilities included preparation of plans and reports documenting existing conditions along with development of plans and details for the proposed rehabilitation.

**Route 95 Bridge Rehabilitation, West Greenwich-Exeter, RI:** Engineer in charge of the field inspection of five highway bridges. Responsibilities also included; preparation of plans and reports documenting existing conditions, seismic analysis, fatigue analysis and deck replacement design.

**Pontiac Bridge, Route 5, Warwick, RI:** Performed field inspection of a two-span steel stringer bridge spanning the Pawtuxet River. Responsibilities included preparation of plans and reports documenting existing conditions along with development of plans and details for the proposed rehabilitation.

**Jamestown Verrazzano Bridge, Route 138, Kingston, RI:** Performed field inspection of the twenty-nine span trestle approach spanning Narragansett Bay and assisted with the inspection of the main span segmental box girder. Responsibilities also included preparation of plans and reports documenting existing conditions.



## **Martin J. Pierce, PE** *Lead Structural Design Engineer*

**Moffett Mill Rehabilitation, Lincoln, RI:** Responsibilities included the design and plan development of an elevated timber walkway and railing in conjunction with a new two-span timber pedestrian bridge, as part of a nature trail located adjacent to the historic Moffett Mill and Chase Farm Park. The design utilized helical piles to support the elevated timber walkway to minimize the disturbance to the surrounding wetland areas.

**Woonsocket Boat Launch, Woonsocket RI:** Responsibilities included the design and plan development of a pile supported timber framed boat launch and dock with an adjacent timber framed overlook area located along the Blackstone River. The design utilized timber piles socketed in bedrock to support the main framing of the dock and overlook area. The design also incorporated timber framed ramps which provided access from the dock to the boat launches.

**Quonset Point Davisville, Pier No. 1, North Kingstown RI:** Performed Construction supervision of the repairs to the timber piles supporting carrier Pier No.1 in Narragansett Bay. The repair details included wrapping the existing piles with neoprene jackets filled with cast-in-place concrete in addition to driving new timber piles to shore up the existing pier.

**Quonset Point Davisville, Pier No. 2, North Kingstown RI:** Performed the design and plan development along with the construction supervision relating to the steel cofferdam repairs located beneath carrier Pier No. 2. The design included pouring a reinforced concrete girdle against the cofferdams below Narragansett Bay to repair and prevent further deterioration. Repairs also included the use of low density concrete to fill the voids beneath the concrete deck of the pier.

**Wickford Boat Ramp, Wickford, RI:** Responsibilities included the design and plan development of a pile supported floating dock and timber ramp located in Wickford Cove at Wilson Park. The design incorporated a timber framed ramp and hand railing to provide access to the floating dock.

**Dexter Corporation , Windsor Locks, CT:** Engineer in charge of structural steel framing design and detailing for a 32,000-square foot, three-story pile supported industrial building. Performed finite element analysis of second floor framing, under various static and moving load combinations to predict live load deflections beneath sensitive machinery.

**Advanced Wastewater Treatment Facility, Douglas, MA:** Engineer in charge of design and plan development of a 1.25 Million Gallon Sequence Batch Reactor, utilizing Finite Element modeling. Responsible for the design and plan development of a 3,500 square foot concrete masonry Process Building and reinforced concrete Vortex Grit Chamber. Responsible for the design and plan development of a 2,000 square foot concrete masonry Utility Building.

**Wastewater Treatment Facility Upgrades, Warwick, RI:** Assisted with design and plan development for a new 400,000-gallon reinforced concrete chlorine contact tank. The project also included structural modifications to an existing 250,000-gallon reinforced concrete chlorine contact tank. Participated in the design and detailing of two new reinforced concrete aeration tanks totaling 3 million gallons capacity.

# ARCHITECT – ENGINEER QUALIFICATIONS

## PART I - CONTRACT-SPECIFIC QUALIFICATIONS

### A. CONTRACT INFORMATION

1. TITLE AND LOCATION *(City and State)*

**Design/Build Services for the Replacement of the Laurel Avenue Br. No. 397, Coventry, RI**

2. PUBLIC NOTICE DATE

April 22, 2011

3. SOLICITATION OR PROJECT NUMBER

**Bid # 7448315**

### B. ARCHITECT-ENGINEER POINT OF CONTACT

4. NAME AND TITLE

**Yihui Peter Wu, PE, PhD –Project Manager**

5. NAME OF FIRM

**Maguire Group Inc.**

6. TELEPHONE NUMBER

401.272.6000

7. FAX NUMBER

401.272.9185

8. E-MAIL ADDRESS

ywu@maguiregroup.com

### C. PROPOSED TEAM

*(Complete this section for the prime contractor and all key subcontractors.)*

	(Check)			9. FIRM NAME	10. ADDRESS	11. ROLE IN THIS CONTRACT
	PRIME	J-V PARTNER	SUBCON- TRACTOR			
a.	✓			<b>Maguire Group Inc.</b> <input checked="" type="checkbox"/> CHECK IF BRANCH OFFICE	225 Chapman Street, 4 <sup>th</sup> Floor Providence, RI 02905	Lead Bridge Design
b.				<input type="checkbox"/> CHECK IF BRANCH OFFICE		
c.				<input type="checkbox"/> CHECK IF BRANCH OFFICE		
d.				<input type="checkbox"/> CHECK IF BRANCH OFFICE		
e.				<input type="checkbox"/> CHECK IF BRANCH OFFICE		
f.				<input type="checkbox"/> CHECK IF BRANCH OFFICE		

D. ORGANIZATIONAL CHART OF PROPOSED TEAM

*(Attached)*

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> Yihui P. Wu	<b>13. ROLE IN THIS CONTRACT</b> Project Manager	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 18	b. WITH CURRENT FIRM 8

**15. FIRM NAME AND LOCATION (City and State)**  
Maguire Group Inc., New Britain, CT

<b>16. EDUCATION (DEGREE AND SPECIALIZATION)</b> Post-Doctoral, Structural Engineering, SUNY at Buffalo, 1997 / Doctor of Engineering, China Academy of Railway Sciences, 1992 / Master of Science of Civil Engineering, Southwestern Jiaotong University, China, 1986 B.S. Civil Engineering, South Western Jiaotong University, China, 1983	<b>17. CURRENT PROFESSIONAL REGISTRATION (STATE AND DISCIPLINE)</b> PE Structural CT #0027925, RI #8341, MA 48038, NH #13051, FL #71123
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**18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.)**  
Member of EERI (Earthquake Engineering Research Institute), Member of ASCE (American Society of Civil Engineers), BSCE (Boston Society of Civil Engineers), NBI certificate for bridge inspection, MBTA ROW Safety Certificate, MBCR ROW Safety Certificate, OSHA 10 hour Construction Safety Certificate OSHA-10 Hour Construction Safety & Health, NHI Safety Inspection of In-Service Bridge, Member of ASCE, EERI. 25 technical papers published.

**19. RELEVANT PROJECTS**

	(1) TITLE AND LOCATION (City and State)	(2) YEAR COMPLETED	
		PROFESSIONAL SERVICES	CONSTRUCTION (If applicable)
a.	<b>Design-Build Oversight on Phillipston Bridge Replacement (Route 2 over 2A), Phillipston, MA</b>	2010 - 2011	2010
	<b>(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE</b> <input checked="" type="checkbox"/> Check if project performed with current firm Project Manager, owner's representative for the review of design submissions, construction project management, construction contract package, quality assurance/control programs and submissions, recording submittals, and part-time inspection services. The structure replacement utilized Accelerated Bridge Construction (ABC) methods to replace a 2 lane structure carrying Route 2 over Route 2A increasing the vertical clearance over Route 2A. The ABC methods allowed for the entire construction to be completed in one construction season, required only a 3 day roadway closure for replacement of the superstructure, and minimized construction related impacts. Total project value \$3,100,000.		
b.	<b>I-195 Relocation Project Providence, RI</b>	2001-2011	On Going
	<b>(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE</b> <input checked="" type="checkbox"/> Check if project performed with current firm Structural Project Manager responsible for all bridge LRFR Rating, budget, schedule, Quality Control and Assurance. Project Principal Engineer responsible for the bridge design, detail, and contract bid documents. Also responsible for shop drawing review and approval, RFI Response, and construction coordination. The \$670 million I-195 relocation project includes 16 new bridges and 61 new walls including a network arch, curved steel box girders, concrete box beams, MSE and cast-in-place retaining walls. Foundation types include drilled shafts, H-piles and spread footings.		
c.	<b>RIDOT, Pawtucket Rl. George St. Bridge, Garden St. Bridge and Pine St. Bridge Replacement</b>	2010	NA
	<b>(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE</b> <input checked="" type="checkbox"/> Check if project performed with current firm Principal Engineer, responsible for the quality control and review for the project deliverables. These bridge replacement and rehabilitations are part of RIDOT \$125M Pawtucket River Bridge 550 Replacement Project. The replacement design includes complete replacement of George St. Bridge, deck and pier replacement of Pine St. and Garden St. Bridges.		
d.	<b>RIDOT, I-195 Relocation Project, India Point Park Pedestrian Bridge, Providence, RI</b>	2007	2009
	<b>(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE</b> <input checked="" type="checkbox"/> Check if project performed with current firm Principal Engineer responsible for bridge design from 30% to completion, and supervision of bridge and retaining wall design. Responsible for pier column design, south abutment design, rib beam and deck design, concrete joint reinforcement detailing in accordance with seismic requirements, and bridge construction coordination with client and contractor. This \$26-million project included a 2-span pre-cast box beam bridge across I-195, a 2-span cast-in-place concrete rigid frame arch, and a 1,800-ft-long retaining wall. It is part of the Providence India Point Park reconstruction, with maximum 17-ft soil depth, trees and landscape structures on the bridge.		
e.	<b>Route 8 over the B&amp;M Railroad and Hoosic River (Hadley Overpass) Bridge Rehabilitation, North Adams, MA:</b>	2008-2010	On going
	<b>(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE</b> <input type="checkbox"/> Check if project performed with current firm Project Manager responsible for the project construction-phase service management, contract amendment, MOD and invoices, construction coordination, shop drawing approval and RFI response, contract budget, schedule and quality assurance and control. This \$25-million project includes replacing most of the 14 span steel girder and pier repair, truss span rehabilitation. The bridge is situated in the downtown area of North Adams; traffic is open during the construction, and staging construction is anticipated for this project.		

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> David B. Freeman, PE	<b>13. ROLE IN THIS CONTRACT</b> QA / QC	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 30	b. WITH CURRENT FIRM 24

**15. FIRM NAME AND LOCATION** *(City and State)*  
Maguire Group Inc., Providence, Rhode Island

<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> BS/1979/Civil Engineering Lehigh University	<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i> Professional Engineer FL 67259, MA 39893, NJ GE29476, NY 631801, RI 4870
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**18. OTHER PROFESSIONAL QUALIFICATIONS** *(Publications, Organizations, Training, Awards, etc.)*  
**Organizations:** American Society of Civil Engineers, Institute of Transportation Engineers, Professional Engineers *Engineer of the Year – 2005*

**19. RELEVANT PROJECTS**

<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Design and Oversight, Iway Relocation of Interstate 195, RIDOT, Providence, RI	<b>(2) YEAR COMPLETED</b>	
	PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>

**(3) BRIEF DESCRIPTION** *(Brief scope, size, cost, etc.)* AND **SPECIFIC ROLE**  **Check if project performed with current firm**  
a. As Senior Project Manager, was responsible for supervising the preparation of the Final Design for this \$450-million project that will run through 2012. Supervision involved overseeing the efforts of a dozen subconsultants and over 30 Maguire staff. The project includes relocating almost two miles of urban interstate including the I-195 / I-95 interchange through Providence. There are 16 bridges, the largest of which is a 900-foot-long, 10-lane-wide crossing of the Providence River. The construction sequence calls for 15 construction contracts ranging in value from \$1 million to \$85 million and an additional five demolition contracts. Final Design included a complete scope of engineering efforts on the part of Maguire Group and subconsultants that needed to be managed to bring the project to fruition.

<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Traffic Analysis and Road Improvement Design, Routes 146 and 116 Interchange, RIDOT, Lincoln, RI	<b>(2) YEAR COMPLETED</b>	
	PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>

**(3) BRIEF DESCRIPTION** *(Brief scope, size, cost, etc.)* AND **SPECIFIC ROLE**  **Check if project performed with current firm**  
b. Route 116 is a major corridor in Lincoln, Cumberland and part of Smithfield, RI; Route 146 runs from Providence north to the Massachusetts border. The Routes 116-146 interchange was the first cloverleaf interchange to be constructed in RI, and major reconstruction began in 2005. As Project Manager, Mr. Freeman studied reconstruction alternatives that included the analysis and prediction of future traffic conditions for the various alternatives. Also prepared the preliminary design for improvements to 2.5 miles of Route 146 and two miles of Route 116.

<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> I-95 Ramps Project, Providence, RI	<b>(2) YEAR COMPLETED</b>	
	PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>

**(3) BRIEF DESCRIPTION** *(Brief scope, size, cost, etc.)* AND **SPECIFIC ROLE**  **Check if project performed with current firm**  
c. Mr. Freeman served as Project Manager as a subconsultant on a design team to improve access into Downtown Providence to support major retail development. Design responsibilities included reconstruction of a freeway terminal at the interface with arterial streets, accommodation of significant pedestrian movements, interconnected signal design, and preparation of documentation to gain approval from the Capital Center Commission, a quasi-public design review committee.

<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> River Relocation and Street Reconstruction, Memorial Boulevard Extension, Providence, RI	<b>(2) YEAR COMPLETED</b>	
	PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>

**(3) BRIEF DESCRIPTION** *(Brief scope, size, cost, etc.)* AND **SPECIFIC ROLE**  **Check if project performed with current firm**  
d. As a Project Engineer, Mr. Freeman's responsibilities included preparation of street reconstruction plans and specifications for the third, fourth and fifth contracts of a six-contract series to relocate rivers and construct a major boulevard and waterfront pedestrian esplanade through Providence. His duties included urban street design, coordinating with the bridge design and utility relocation efforts, detailing maintenance and protection of traffic plans, and support during construction.

<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Environmental Impact Statement – Improvements to I-195, Providence, RI	<b>(2) YEAR COMPLETED</b>	
	PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>

**(3) BRIEF DESCRIPTION** *(Brief scope, size, cost, etc.)* AND **SPECIFIC ROLE**  **Check if project performed with current firm**  
e. Responsible for preparing an Environmental Impact Statement (EIS) for the relocation of I-195 through downtown Providence. The EIS investigated various alternatives for improvements to a major urban interstate and an interchange with I-95, which included development of future traffic patterns, analysis of levels of service, change of access request, and a determination of the feasibility of the alternatives in terms of geometric adequacy and estimated construction costs. In addition, it fully evaluated the environmental and social impacts, urban design issues, and 4f/6f and Section 106 issues.

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> Martin J. Pierce, PE	<b>13. ROLE IN THIS CONTRACT</b> Lead Bridge / Structural Engineer	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 16	b. WITH CURRENT FIRM 8

**15. FIRM NAME AND LOCATION** *(City and State)*  
Maguire Group Inc. Providence, RI

<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> BS / 1996 / Structural Engineering, University of Massachusetts, Dartmouth MS / 2003 / Structural Engineering, University of Rhode Island	<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i> Professional Engineer – RI 7446
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**18. OTHER PROFESSIONAL QUALIFICATIONS** *(Publications, Organizations, Training, Awards, etc.)*  
OSHA -10 Hour Construction Safety & health, OSHA Permit Required Confined Space Entry, AMTRAK On Track Safety Training for Contractors

**19. RELEVANT PROJECTS**

a.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> I-195 Relocation, Providence, RI: Contracts 6, 6A, 10 & 11 – Ramp NE, Ramp WS, Ramp WES, Ramp SE, Ramp PS, Clifford, Oxford, Public and O'Connell Street Bridges	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2009 - 2011	CONSTRUCTION <i>(If applicable)</i> 2010 - 2011
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Assisted in the design, plan development and preparation of contract documents. Responsibilities included designing various elements relating to multiple span pre-stressed box beams, curved steel girder box beams, curved steel plate girders, substructure abutments, multiple column pier bents, hammerhead pier caps and columns, shallow and deep foundations, along with cantilever and mse retaining walls. Design forces and deformations obtained from the creation and analysis of multiple finite element models.		
b.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> I-195 Relocation Contract 9 Providence, Rhode Island	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2003-2008	CONSTRUCTION <i>(If applicable)</i> 2007-2008
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Performed duties as structural engineer and construction phase services for the replacement of an existing single girder pedestrian bridge span over Route I-195 in Providence, RI. The structure replacement consisted of 2-span continuous prestressed concrete box girder bridge, 2-span rigid frame pedestrian overpass supporting India Point Park over Route I-195 and India St. The bridge incorporated unique architectural elements including complex beam and column configurations, stainless steel, glass block, and granite lantern assemblies, concrete and granite planters and seat walls, and an ADA compliant walkway through the park. Total project value \$26,000,000.		
c.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Garden Street Bridge, Pawtucket Rhode Island	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2010	CONSTRUCTION <i>(If applicable)</i> N/A
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm This structure is a 4 span steel I-girder bridge which carries Garden Street over Route 95. The bridge consists of (9) cover plated I girders and has a total width of 54 feet. Contract consisted of superstructure and intermediate pier replacement. Responsibilities included the substructure design of the three intermediate multi-column pier caps and spread footings along with the design of seismic isolation bearings, used to reduce forces on the new piers.		
d.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Pine Street Bridge, Pawtucket Rhode Island	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2010	CONSTRUCTION <i>(If applicable)</i> N/A
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm This structure is a 4 span steel I-girder bridge which carries Pine Street over Route 95. The bridge consists of (9) cover plated I girders and has a total width of 54 feet. Contract consisted of superstructure and intermediate pier replacement. Responsibilities included the substructure design of the three intermediate multi-column pier caps and spread footings along with the design of seismic isolation bearings, used to reduce forces on the new piers.		
e.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Wilbur Road Bridge No. 415, Lincoln Rhode Island	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2011	CONSTRUCTION <i>(If applicable)</i> Ongoing
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm This structure is a 2 span steel girder bridge which carries Wilbur Road over Route 146. The bridge consists of 8 girders in cross section and has a total width of 42 feet. Contract consisted of superstructure replacement. Responsibilities included the design of the reinforced concrete deck, new elastomeric bearings and camber & deflection.		

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> Matthew J Weidele	<b>13. ROLE IN THIS CONTRACT</b> Lead Structural Engineer	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 14	b. WITH CURRENT FIRM 8

**15. FIRM NAME AND LOCATION** *(City and State)*  
Maguire Group Inc., Providence, RI

<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> BS/1997/Civil Engineering Worcester Polytechnic Institute	<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i> Professional Engineer, RI 7861 – Structural Professional Engineer, FL 70244 – Structural
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**18. OTHER PROFESSIONAL QUALIFICATIONS** *(Publications, Organizations, Training, Awards, etc.)*  
OSHA-10 Hour Construction Safety & Health, OSHA-Permit Required Confined Space Entry, AMTRAK – On-Track Safety Training for Contractors

**19. RELEVANT PROJECTS**

a.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Route 2 over 2A Superstructure Replacement, Phillipston, MA	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2009 - 2011	CONSTRUCTION <i>(If applicable)</i> Ongoing
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> <b>AND SPECIFIC ROLE</b> Performed review of the work performed by the design-build entity for the replacement of a 4-lane highway bridge as part of the Accelerated Bridge Program. Responsible for the review of design submissions, for the proposed bridge work with required turn-around times as short as 24 hours.	<input checked="" type="checkbox"/> Check if project performed with current firm	
b.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Route I-195 Relocation, Providence, RI	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2009-2011	CONSTRUCTION <i>(If applicable)</i> 2010-2011
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> <b>AND SPECIFIC ROLE</b> Responsible for the design and detailing of a 2-span continuous prestressed concrete box girder, 2-span rigid frame pedestrian overpass. Developed a 3-dimensional, finite element model incorporating unique design elements to satisfy architectural requirements including stainless steel, glass block and lightweight concrete lantern assemblies, complex column and beam configurations, continuous concrete and granite planters over precast spans, and a park featuring an ADA compliant walkway. Responsible for the design and detailing of post-tensioned hammerhead concrete piers. Designed and detailed architectural precast fascia panel system and moment slab with TL-4 barrier for MSE Walls. Responsible for quantity and estimate development for multiple contracts. Created or revised contract documents including job specific specifications and plans.	<input checked="" type="checkbox"/> Check if project performed with current firm	
c.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> I-95 Service Roads, Garden, Pine, and George Street Bridges, Pawtucket, RI	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2008 - present	CONSTRUCTION <i>(If applicable)</i> 2010 - present
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> <b>AND SPECIFIC ROLE</b> Performed duties as lead structural engineer and task manager for design and construction phase services for the replacement of 1 and rehabilitation of 2 existing bridges. The bridge rehabilitations consist of the replacement of pier substructures, bridge decks and parapets, and bearings for four span steel stringer bridges carrying service roads over Route I-95. The replacement consists of a 2-span continuous prestressed butted box beam structure along with new cast-in-place abutments and piers carrying a service road over Route I-95. Construction was detailed to be phased in order to maintain all travel lanes active along I-95. Total project value \$125,000,000.	<input checked="" type="checkbox"/> Check if project performed with current firm	
d.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> I-195 Relocation, Contract 9, Providence, RI	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2003-2008	CONSTRUCTION <i>(If applicable)</i> 2007-2008
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> <b>AND SPECIFIC ROLE</b> Performed duties as structural engineer and construction phase services for the replacement of an existing single girder pedestrian bridge span over Route I-195 in Providence, RI. The structure replacement consisted of 2-span continuous prestressed concrete box girder bridge, 2-span rigid frame pedestrian overpass supporting India Point Park over Route I-195 and India St. The bridge incorporated unique architectural elements including complex beam and column configurations, stainless steel, glass block, and granite lantern assemblies, concrete and granite planters and seat walls, and an ADA compliant walkway through the park. Total project value \$26,000,000.	<input checked="" type="checkbox"/> Check if project performed with current firm	
e.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Mapleville Main St. Bridge No. 337, Burrillville, RI	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2002-2003	CONSTRUCTION <i>(If applicable)</i> 2003
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> <b>AND SPECIFIC ROLE</b> Performed duties as structural engineer for the design and detailing of a new 30' span by 43' wide structure to replace the existing deteriorated structure. Responsible for the design of a cofferdam structure to retain the Chepachet River during construction. Maintained aesthetic requirements through coordination with the RIDOT Historical Preservation Specialist. Developed specifications for the significant number of specialty items associated with the new structure. Total project value \$1,000,000.	<input type="checkbox"/> Check if project performed with current firm	

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> Skyler D. Mills, PE	<b>13. ROLE IN THIS CONTRACT</b> Lead Highway Engineer	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 19	b. WITH CURRENT FIRM 11

**15. FIRM NAME AND LOCATION** *(City and State)*  
Maguire Group, Inc., Providence, RI

<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> AAS/1980/Marine Technology/ Cape Fear Community College BS/1991/Civil Engineering/ New Jersey Institute of Technology (Summa Cum Laude)	<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i> Professional Engineer: MA 42536, NJ 24GE04070500, RI 6896
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**18. OTHER PROFESSIONAL QUALIFICATIONS** *(Publications, Organizations, Training, Awards, etc.)*  
American Society of Civil Engineers

**19. RELEVANT PROJECTS**

	(1) TITLE AND LOCATION <i>(City and State)</i>	(2) YEAR COMPLETED	
		PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>
a.	<b>Roadway Design, Iway Project, Rhode Island Dept. of Transportation (RIDOT), Providence, R</b>	2000-2009	2010-2011
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Project Engineer, design of Providence I-95 and I-195 mainlines, ramps and city streets, including horizontal and vertical alignments, superelevation transitions, volume calculations, cross sections, grading, utilities, traffic control plans, estimating, barrier design and slope treatments. Extensive use of AutoCAD 2008 and Eagle Point civil engineering modules such as RoadCalc and Surface Modeling. Wrote general and project-specific AutoLISP routines for expediting design and drafting. Coordinated RIDOT electronic bidding using Quest Professional, responsible for I-195 Contract 9 addenda.		
b.	<b>Worcester Gateway Park Road Work, Worcester, MA</b>	2005	2011
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Design of the Worcester Gateway project involving roadways, including horizontal alignment, cross sections, signing, pavement marking, traffic control, right-of-way, estimates and ADA compliance.		
c.	<b>Roadway Design, Design of Routes 146 and 116 Northeast Quadrant Interchange, RIDOT, Lincoln, RI</b>	2005	2007
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Tasks included cross sections, grading, drainage, utilities, surface modeling, and barrier design.		
d.			
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm		
e.			
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm		

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> William G. Young, PE	<b>13. ROLE IN THIS CONTRACT</b> Environmental Engineer	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 14	b. WITH CURRENT FIRM 10
<b>15. FIRM NAME AND LOCATION</b> <i>(City and State)</i> Maguire Group Inc. Providence, RI			
<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> BS/1998/Civil Engineering Associate of Science/1994 Pending/Certificate in Engineering Management		<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i> Professional Engineer - Civil, RI 7867	
<b>18. OTHER PROFESSIONAL QUALIFICATIONS</b> <i>(Publications, Organizations, Training, Awards, etc.)</i> Past President, RI Section of the American Society of Civil Engineers			

**19. RELEVANT PROJECTS**

a.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Pawtucket/Mineral Spring Avenue ADA Compliance, East Providence/North Providence, RI (RIDOT)	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2002	CONSTRUCTION <i>(If applicable)</i> 2003
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE Lead Project Engineer responsible for the upgrade/addition of highway pavement stormwater collection systems, reconstruction of sidewalks, wheelchair ramps, and crosswalks. The intent of the project was to promote handicap accessibility as well as pedestrian safety along 1.7 miles of state highway. The project also included the re-alignment of the Pawtucket Ave./Bullock's Point Ave. Intersection to address safety concerns.	<input type="checkbox"/> Check if project performed with current firm	
b.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> U.S. Route 6 Improvements, Johnston/Providence, RI (RIDOT)	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES ongoing	CONSTRUCTION <i>(If applicable)</i> n/a
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE Project Engineer responsible for electronic contract document compilation for improvements to 3.6 miles of this major East/West four-lane highway. The reconstruction of six bridges was also included within the contract. This was one of the first projects RIDOT advertised with its new electronic bidding system, and the very first advertised under an agreement with Maguire Group Inc.	<input type="checkbox"/> Check if project performed with current firm	
c.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Bridge Replacement Br. No. H-06-003=H-07-003 (Concrete), Broadway/Winter Streets over the Indian Head River, Hanover-Hanson, MA (MassDOT Accelerated Bridge Program)	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2002	CONSTRUCTION <i>(If applicable)</i> 2003
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE Project Consultant/Sales Engineer (with Contech Construction Products, Inc.) responsible for identifying and presenting a successful value engineering proposal on behalf the winning contractor, AA Will Corporation. The solution contained within the original contract documents was for an "in kind" replacement of a cast-in-place 21' span reinforced concrete arch on a 19 degree skew to the roadway. Due to the dewatering complexities associated with pouring new foundations within the river all the way to bedrock as well as required wetland replication for compensation, AA Will Corp. preferred to do all foundation work outside of the existing channel and avoid wetland impacts/replication altogether. I presented a 42' span (no skew) ConSpan® complete pre-cast bridge system with TL-4 crash rated headwalls and wingwalls. I purposely matched the existing hydraulic opening beyond the 10 year storm event by leaving the historic granite abutments in place. This solution reduced several environmental as well as socioeconomic impacts by staying out of the wetlands as well as reducing the construction time from 18 months to 7 months while completely eliminating phased construction which included a long term signal controlling alternating one-way traffic. The original agreed upon contract amount was \$1,392,000, and AA Will Corporation provided a contract credit to MassDOT for \$303,000, or an overall savings to the taxpayers of 12%.	<input type="checkbox"/> Check if project performed with current firm	
d.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Route 2 – South County Trail ADA Compliance, Charlestown, RI (RIDOT)	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2005	CONSTRUCTION <i>(If applicable)</i> 2005
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE Project Engineer partially responsible for the design and preparation of contract documents for 0.8 miles of new sidewalk. The project is being undertaken to promote safer pedestrian and handicap access along this rural stretch of Route 2. RIDOT had asked Maguire to work in close coordination with the Indian Health Service on this project since the Narragansett Indian Tribe is constructing a new domestic water main within the same right-of-way.	<input type="checkbox"/> Check if project performed with current firm	
e.	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i> Reconstruction of Route 3/Cowesett Avenue, West Warwick, RI (RIDOT)	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES 2004	CONSTRUCTION <i>(If applicable)</i> 2005
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE Project Engineer partially responsible for the design of improvements along 1.8 miles of state highway. The project included drainage, pavement base course, sidewalk, and traffic signal redesign and reconstruction. Close coordination with RIDOT's Construction Bureau as well the selected contractor was required during construction due to the limited right-of-way and underground utility congestion..	<input type="checkbox"/> Check if project performed with current firm	

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> Jennifer Ann James, PS		<b>13. ROLE IN THIS CONTRACT</b> Environmental Scientist		<b>14. YEARS EXPERIENCE</b>	
				a. TOTAL 10	b. WITH CURRENT FIRM 6
<b>15. FIRM NAME AND LOCATION</b> <i>(City and State)</i> Maguire Group Inc. Foxborough, MA					
<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> BS/2001/Wildlife Biology University of Rhode Island			<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i>		
<b>18. OTHER PROFESSIONAL QUALIFICATIONS</b> <i>(Publications, Organizations, Training, Awards, etc.)</i> Wildlife Society Member, Society of Wetland Scientists New England, Invasive Plant Group					

**19. RELEVANT PROJECTS**

	(1) TITLE AND LOCATION <i>(City and State)</i>	(2) YEAR COMPLETED	
		PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>
a.	Permitting and Delineation, Dexter Road, East Providence, RI		
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Delineated coastal vegetated wetlands and also inland vegetated wetlands. Also delineated areas of critical concern. Prepared permitting for the Rhode Island Department of Environmental Management (RIDEM).		
b.	Route 146/116 Site Investigations, Lincoln RI	2003	2005
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Conducted field investigations of wetland remediation and development. Surveyed the area daily to ensure that the wetlands were being protected and managed appropriately.		
c.	Phase I Site Investigation Town of North Kingstown, Kingston, RI	2003	NA
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Prepared a Phase I Environmental Assessment for the Town of North Kingstown, this assessment determined the potential for any hazardous materials or oil release.		
d.	Subsurface Investigation and Site Investigation Report, East Providence, RI	2007	NA
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Coordinated and provided field oversight of all subsurface investigations and prepared a Site Investigation Report. Activities included subcontractor engagement, test borings, soil and groundwater remediation, soil and groundwater sampling, and laboratory analysis.		
e.	Wetland Delineation and Permitting, Nickerson State Park, Brewster, MA	2002	NA
	(3) BRIEF DESCRIPTION <i>(Brief scope, size, cost, etc.)</i> AND SPECIFIC ROLE <input checked="" type="checkbox"/> Check if project performed with current firm Delineated freshwater wetlands throughout a 1900-acre state park. Prepared Eastern Box Turtle work plan for Natural heritage for the protection of the endangered Eastern Box Turtle. Prepared a Notice of Intent for the local conservation commission and for the MA DEP and attended public meetings. Coordinated and permitted a sewer replacement and electrical line replacement project for the park with MA Historical, MADEP, the local Conservation Commission and Natural Heritage and Endangered Species Program.		

**E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT**

*(Complete one Section E for each key person.)*

<b>12. NAME</b> Jeffrey Lemay	<b>13. ROLE IN THIS CONTRACT</b> Hydrology Engineer	<b>14. YEARS EXPERIENCE</b>	
		a. TOTAL 10	b. WITH CURRENT FIRM 10
<b>15. FIRM NAME AND LOCATION</b> <i>(City and State)</i> Maguire Group Inc., New Britain, Connecticut			
<b>16. EDUCATION</b> <i>(DEGREE AND SPECIALIZATION)</i> BS/1998/Civil Engineering University of Connecticut		<b>17. CURRENT PROFESSIONAL REGISTRATION</b> <i>(STATE AND DISCIPLINE)</i> Engineer In Training CT/1999/Civil Engineering	
<b>18. OTHER PROFESSIONAL QUALIFICATIONS</b> <i>(Publications, Organizations, Training, Awards, etc.)</i>			

**19. RELEVANT PROJECTS**

	<b>(1) TITLE AND LOCATION</b> <i>(City and State)</i>	<b>(2) YEAR COMPLETED</b>	
		PROFESSIONAL SERVICES	CONSTRUCTION <i>(If applicable)</i>
a.	<b>Eagle Square, Providence, Rhode Island</b>	2003	2005
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.) AND SPECIFIC ROLE</i> <input checked="" type="checkbox"/> <b>Check if project performed with current firm</b> Design of horizontal and vertical geometry for proposed shopping center parking lots and driveways. Other work includes design of storm sewer systems and hydraulic analysis for the Woonasquatucket River. Hydraulic Analysis performed using HEC-2 software and HEC-RAS software. Prepared Hydraulic Analysis Report. Prepared contract drawings using AutoCAD software. Coordination with state and local officials as well as sub-consultants		
b.	<b>Floodplain Evaluation and Flood Mitigation Johnston, RT</b>	2006	n/a
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.) AND SPECIFIC ROLE</i> <input checked="" type="checkbox"/> <b>Check if project performed with current firm</b> Performed hydraulic river analysis for flood mitigation \ flood proofing within FEMA regulated 100yr floodplain. Analysis performed using HEC-RAS software. Prepared Hydraulic Analysis Report. Coordination with civil design team to select flood proofing measures. Provided assistance for preparation of permit application.		
c.	<b>Bemis Street Bridge Replacement, Plymouth, CT</b>	2003	2004
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.) AND SPECIFIC ROLE</i> <input checked="" type="checkbox"/> <b>Check if project performed with current firm</b> Performed hydraulic analysis and scour analysis for proposed bridge replacement. Analysis performed using HEC-2 software, HEC-RAS software. Prepared Hydraulic Analysis Report. Assisted in the design of storm drainage and roadway geometry. Assisted in preparation of CTDEP permit applications. Cost: \$250,000		
d.	<b>Chapel Street Bridge Rehabilitation, New Haven, CT</b>	2004	2005
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.) AND SPECIFIC ROLE</i> <input checked="" type="checkbox"/> <b>Check if project performed with current firm</b> Design of horizontal and vertical roadway geometry associated with a bridge superstructure replacement. Prepared contract drawings and specifications.		
e.	<b>West Washington St. Bridge Replacement, Bristol, CT</b>	2005	N/A
	<b>(3) BRIEF DESCRIPTION</b> <i>(Brief scope, size, cost, etc.) AND SPECIFIC ROLE</i> <input checked="" type="checkbox"/> <b>Check if project performed with current firm</b> Performed hydraulic analysis and scour analysis for proposed bridge replacement. Analysis performed using HEC-2 software, HEC-RAS software. Prepared Hydraulic Analysis Report. Involved in the design of storm drainage and roadway geometry. Assisted in preparation of CTDEP permit applications. Cost: \$200,000		

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**

1

**21. TITLE AND LOCATION** *(City and State)*

I-195 Relocation  
The Iway Project  
Providence, RI

**22. YEAR COMPLETED**

**PROFESSIONAL SERVICES**  
2010

**CONSTRUCTION** *(If applicable)*  
2013

**23. PROJECT OWNER'S INFORMATION**

**a. PROJECT OWNER**  
RIDOT

**b. POINT OF CONTACT NAME**  
Kazem Farhoumand, PE

**c. POINT OF CONTACT TELEPHONE NUMBER**  
401.222.2023 ext 4104

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

Maguire Group is the lead consultant and program manager for relocation of I-195 in Providence. As such, Maguire has been responsible for the design of the major elements of the project in addition to directing the design activities of 12 subconsultants. Maguire is engaged in all aspects of the program including road and bridge design, permitting, right-of-way procurement, geotechnical and marine engineering.

This is Rhode Island's largest transportation project. The project enhances traffic operations and safety while providing significant redevelopment opportunities in Downtown Providence. It relocates a 1950's era freeway away from the center of the city and vacates over 35 acres of land that can be used to enhance the downtown business district.



The project includes 16 new bridges and several thousand linear feet of new retaining walls. The centerpiece of the project is a 400-ft-long network arch span known as the Iway Bridge. A wide range of bridge types was used, including precast concrete boxes for the short spans, trapezoidal steel boxes for the longer spans, and steel I girders for the local overpasses.

Maguire has been responsible for this project from the onset by developing alternatives, drafting the Environmental Impact Statement, and preparing applications for environmental permits.

**Project Cost:** \$623 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
a.	Maguire Group Inc.	Providence, RI	Prime
b.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
c.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
2

**21. TITLE AND LOCATION** *(City and State)*

Route 2 over 2A Superstructure  
Bridge Replacement, Phillipston, MA

**22. YEAR COMPLETED**

**PROFESSIONAL SERVICES**  
2010

**CONSTRUCTION** *(if applicable)*  
Ongoing

**23. PROJECT OWNER'S INFORMATION**

**a. PROJECT OWNER**  
MassDOT

**b. POINT OF CONTACT NAME**  
Walter Heller, PE

**c. POINT OF CONTACT TELEPHONE NUMBER**  
617.973.8183

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

Maguire was engaged by MassDOT to provide review and construction oversight of the work performed by the design-build entity for the replacement of a 2-lane highway bridge superstructure over another highway. The bridge had been deemed structurally deficient because the superstructure was deteriorated and damaged from truck collisions. The Design-Build team performed design and construction services on the replacement of the bridge superstructure and raising the profile of Rte 2 for improved vertical clearance. To avoid future truck collisions, the new vertical clearance was increased to 16'-6".



The bridge is on the MassDOT Accelerated Bridge Program (which aims to restore and repair structurally deficient bridges across the Commonwealth of Massachusetts) and utilized Accelerated Bridge Construction (ABC) methods. ABC methods are means of rapidly building bridges by building major bridge components apart from their final location and installing them quickly using cranes, gantry systems, or self-propelled motorized transporters (SPMTs). ABC techniques minimize construction-related impacts (such as traffic congestion) by moving much of the work of building bridges out of the roadway. The bridge was constructed in just one construction season (conventional staged construction methods would likely have resulted in a 3-year project) on temporary tower supports over Rte 2A adjacent to the existing bridge, and was moved into place within an 8-day period using "heavy-lift" construction in late October 2010.

Maguire was responsible for review of all design submissions and construction project management for the bridge and highway work; acceptance of the final construction contract package; review of the Design-Build team's quality assurance/quality control program and submissions; maintaining a record of submittals; and providing part-time on-site inspection services.

**Project Cost:** \$3.1 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
a.	Maguire Group Inc.	Providence, RI	Prime
b.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
c.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

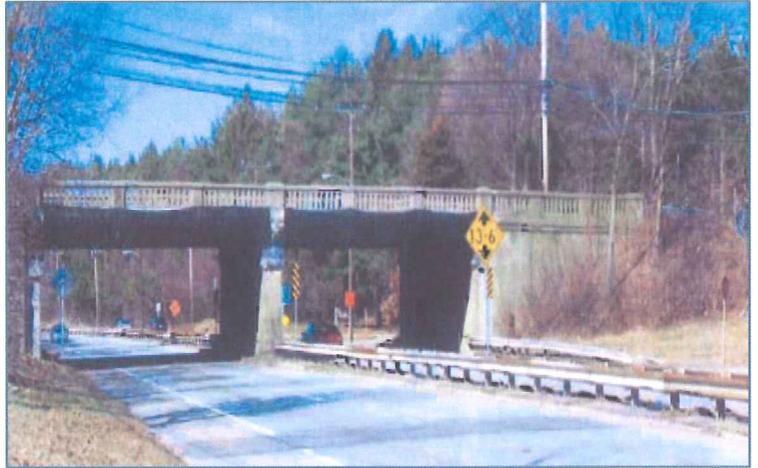
**20. EXAMPLE PROJECT KEY NUMBER**  
3

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> Cedar Street over 2A Route 9 Bridge Replacement D/B, Wellesley, MA	<b>22. YEAR COMPLETED</b>	
	<b>PROFESSIONAL SERVICES</b> Ongoing	<b>CONSTRUCTION</b> <i>(If applicable)</i> NA

<b>23. PROJECT OWNER'S INFORMATION</b>		
<b>a. PROJECT OWNER</b> MassDOT	<b>b. POINT OF CONTACT NAME</b> Walter Heller, PE	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b> 617.973.8183

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

Maguire was engaged by MassDOT to provide review and construction oversight of the work performed by the design-build entity for the replacement of a 2-lane highway bridge superstructure over another highway. It is a 2 span steel girder bridge. The Design-Build team performed design and construction services on the replacement of the bridge superstructure.



The bridge is on the MassDOT Accelerated Bridge Program, and is a Design Build project using Accelerated Bridge Construction (ABC) methods. Bridge beam will be fabricated and supported on temporary tower with deck concrete placed offsite, then Self-propelled motorized transporters (SPMTs) will be used to move the bridge to its location. 72 hour traffic shut down will only allowed for the existing bridge demo and new bridge erection. This bridge construction will be the second MassDOT "Heavy-lift" construction bridge project.

Maguire was responsible for review of all design submissions and construction project management for the bridge and highway work; acceptance of the final construction contract package; review of the Design-Build team's quality assurance/quality control program and submissions; maintaining a record of submittals; and providing part-time on-site inspection services.

**Project Cost:** \$3.45 million

<b>25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT</b>			
<b>a.</b>	(1) FIRM NAME <b>Maguire Group Inc.</b>	(2) FIRM LOCATION <i>(City and State)</i> <b>Providence, RI</b>	(3) ROLE Prime
<b>b.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>c.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>d.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
4

**21. TITLE AND LOCATION** *(City and State)*

Improvements to Interstate  
Route 195  
Contract 6 – Ramps NE, WS & WES

**22. YEAR COMPLETED**

**PROFESSIONAL SERVICES**  
2005

**CONSTRUCTION** *(If applicable)*  
2007

**23. PROJECT OWNER'S INFORMATION**

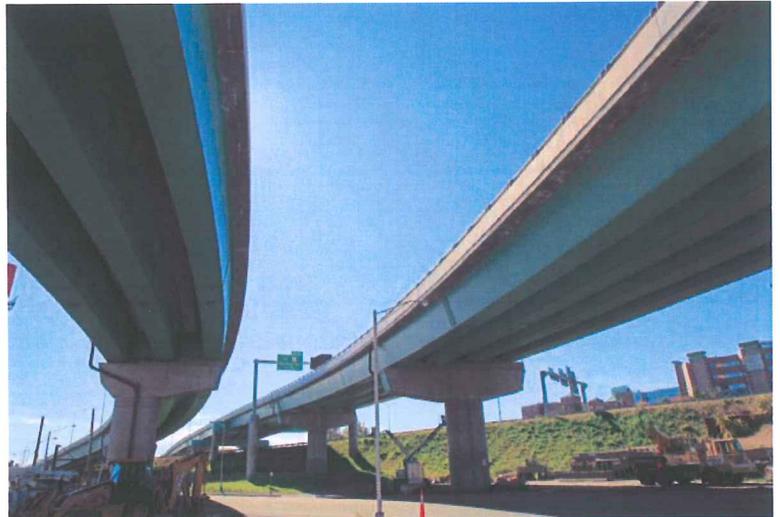
**a. PROJECT OWNER**  
RIDOT

**b. POINT OF CONTACT NAME**  
Kazem Farhoumand, PE

**c. POINT OF CONTACT TELEPHONE NUMBER**  
401.222.2023 ext 4104

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

This construction contract, as part of the Improvements to Interstate Route 195 Project, is comprised of three bridge structures. Ramp NE, which spans from I-95 North to the Providence River (interface with Contract 7) is a 9 span, 1750 foot long viaduct. Ramp WS spanning from US Generating to Rhodes Street is also 9 spans, with a total length of 1570 feet. The Ramp WES bridge is a 3 span, 530 foot long structure across the parking lot of Rhode Island Hospital. All three structures are comprised of multiple curved, steel trapezoidal box girders with cast-in-place concrete deck. Some of these box girders have radii as tight as 440 feet. The substructures are concrete piers and abutments supported on piles.



Constructability was an important part of the design, as the new viaduct spans existing Route 95, which could only be closed to traffic for short periods of time late at night. Location of splices and temporary supports were studied to provide for the steel erection within the site constraints. The design process included coordination with RIDOT and abutters to ensure the construction could be accomplished with minimal disruption to the users and abutters, while not dictating specific means and methods.

Other work included in Contract 6 includes using a portion of Ramp WS highway embankment as part of the hurricane barrier, widening a section of I-95 North, reconstruction of a portion of Allens Avenue, installation of 1000 linear feet of low pressure gas main and 2000 linear feet of high pressure gas main, electrical duct bank relocation, and associated drainage, lighting and other utility work.

**Project Cost:** \$65 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
a.	Maguire Group Inc.	Providence, RI	Prime
b.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
c.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
5

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> Improvements to Interstate Route 195 Contract 8—Ramps SE, WN, WP & NP	<b>22. YEAR COMPLETED</b>	
	<b>PROFESSIONAL SERVICES</b> 2005	<b>CONSTRUCTION</b> <i>(If applicable)</i> 2008

**23. PROJECT OWNER'S INFORMATION**

<b>a. PROJECT OWNER</b> RIDOT	<b>b. POINT OF CONTACT NAME</b> Kazem Farhoumand, PE	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b> 401.222.2023 ext 4104
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**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

There are 4 bridges included in this portion of the Improvements to Interstate Route 195 project. All are designed as multi-span, curved, steel trapezoidal box girders with cast-in-place concrete decks. Ramp WN spans over Franklin Square and is comprised of 3 spans, totaling 400 feet in length. Ramp SE spans across I-95, Eddy St. Allens Ave. and Ramp WS. It is 1600 feet long and consists of 10 spans. Ramp WP spans over Globe and Hospital Street and is a 3-span, 365 foot long structure. Ramp NP is over Ramp WN and is a 7 span, 1000 foot long viaduct. These bridges are founded on piles with cast-in-place concrete abutments and piers. Pier layout was challenging due to extensive utilities, existing streets and structures, and structural inefficiencies due to varying span lengths.



The design included additional challenges due to construction of the ramps over an active interstate highway, and congestion of the construction site. These issues were studied at length and discussed with RIDOT and others, resulting in a suggested construction sequence and tight specifications regarding working hours and limits.

This contract also included design of retaining walls, approach ramps, and embankment construction. A section of Allens Avenue and Eddy Street were reconstructed, as well as the Crary Street Connector.

**Project Cost:** \$49 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
a.	Maguire Group Inc.	Providence, RI	Prime
b.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
c.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
d.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
6

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> Improvements to Interstate Route 195 Contract 9 – Relocated I-195, India Point Park Pedestrian Bridge and Bike Path Underpass	<b>22. YEAR COMPLETED</b>	
	<b>PROFESSIONAL SERVICES</b> 2007	<b>CONSTRUCTION</b> <i>(If applicable)</i> 2009

**23. PROJECT OWNER'S INFORMATION**

<b>a. PROJECT OWNER</b> RIDOT	<b>b. POINT OF CONTACT NAME</b> Kazem Farhoumand, PE	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b> 401.222.2023 ext 4104
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**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

This construction contract includes two bridge structures and six retaining wall structures. One of the important challenges of this contract is the incorporation of many architectural and urban design features into the structures as mitigation for the taking of some of the park land.

The largest structure, the India Point Park Pedestrian Bridge, consists of four spans; the north two spans are precast concrete box beams made continuous for live load and the two south spans are a concrete rigid frame structure. The north spans are 48 feet wide and the rigid frame spans are 110 and 207-feet wide. The entire rigid frame section is covered with plantable soil and has a winding path through the plantings that provides handicapped access. The north spans have planters along the full length of the structure as well. The entire structure is on spread footings.



The other structure, the Bike Path Underpass is a precast concrete culvert on piles. This structure has cast-in-place end walls with ornamental pylons and inlaid tile.

In addition to the bridges, the contract has over 1150 feet of retaining walls including both cast-in-place and MSE retaining walls. Two of the walls have a unique serpentine layout to add to the architectural interest.

**Project Cost:** \$33 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

<b>a.</b>	(1) FIRM NAME <b>Maguire Group Inc.</b>	(2) FIRM LOCATION <i>(City and State)</i> <b>Providence, RI</b>	(3) ROLE Prime, Design
<b>b.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>c.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>d.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

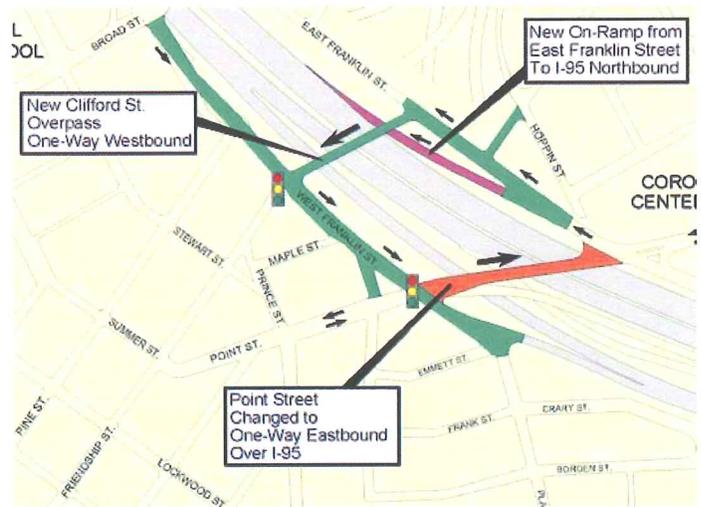
**20. EXAMPLE PROJECT KEY NUMBER**  
7

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> I-195 Interstate Relocation Contract 11 Bridges over I-95 Design and Construction Management	<b>22. YEAR COMPLETED</b>	
	<b>PROFESSIONAL SERVICES</b> 2008	<b>CONSTRUCTION</b> <i>(If applicable)</i> 2010

23. PROJECT OWNER'S INFORMATION		
<b>a. PROJECT OWNER</b> RIDOT	<b>b. POINT OF CONTACT NAME</b> Kazem Farhoumand, PE	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b> 401.222.2023 ext 4104

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

This construction contract, as part of the Improvements to Interstate Route 195 Project, is comprised of two bridge structures. Clifford Street Bridge which spans over interstate 95 is a 2 span, 361 feet steel I girder bridge, Ramp PS Bridge which spans from West Franklin street to Interstate 95 South Bound is a 3 span, 490 feet long horizontally curved steel I girder bridge with cast-in-place concrete deck. Maguire provided full bridge design services, assist the client on construction inspection and management.



**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
a.	Maguire Group Inc.	Providence, RI	Prime
b.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
c.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
d.	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
8

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> I-95 Service Roads Garden, Pine, and George Street Pawtucket, RI	<b>22. YEAR COMPLETED</b>	
	<b>PROFESSIONAL SERVICES</b> 2010	<b>CONSTRUCTION</b> <i>(If applicable)</i> 2013

**23. PROJECT OWNER'S INFORMATION**

<b>a. PROJECT OWNER</b> RIDOT	<b>b. POINT OF CONTACT NAME</b> Kazem Farhoumand, PE	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b> 401.222.2023 ext 4104
----------------------------------	---	--

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

Maguire Group is the lead consultant for the rehabilitation and replacement of 3 bridge structures over Route I-95 in Pawtucket, RI. As such, Maguire has been responsible for the design of the major elements of the project. Maguire is engaged in all aspects of the project including bridge design, permitting, and geotechnical engineering.



The project is a critical element of the Rhode Island Department of Transportation's Bridge 550 Pawtucket River Bridge Replacement, whose total construction cost is estimated at \$115,000,000.

The project includes the rehabilitation of two four span structural steel bridges. The existing steel superstructures are to remain while the concrete deck, sidewalks, parapets, and steel bearings are to be replaced. The new deck detailing eliminates open joints over the piers to extend the service life of the structure. The existing pier substructures are to be replaced. The rehabilitation required that seismic isolation design be performed to ensure that the structure be able to withstand code required seismic affects. Also included is the replacement of a similar 4 span steel structure with a 2 span continuous butted box beam structure. A new pier and abutments were designed to accommodate a revised highway alignment and reduce the number of substructures.

Maguire has been responsible for this project from the onset by developing alternatives, and preparing geotechnical reports and storm water pollution prevention plans.

**Project Cost:** \$15 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

<b>a.</b>	(1) FIRM NAME <b>Maguire Group Inc.</b>	(2) FIRM LOCATION <i>(City and State)</i> <b>Providence, RI</b>	(3) ROLE Prime
<b>b.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>c.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>d.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
9

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> Repair/Replacement Wilbur Road Bridge	<b>22. YEAR COMPLETED</b>	
	PROFESSIONAL SERVICES 2011	CONSTRUCTION <i>(If applicable)</i> Ongoing

23. PROJECT OWNER'S INFORMATION		
<b>a. PROJECT OWNER</b> RIDOT	<b>b. POINT OF CONTACT NAME</b>	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b>

**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

Maguire Group Inc. was hired in January of 2009 to design the superstructure replacement of the Wilbur Road Bridge. The structure had low clearance and had been hit several times in the past few years. Maguire's previous contract was to design repairs and pier and abutment modifications to raise the bridge. When the bridge was hit again, the decision was made to raise and replace the superstructure. This project was expected to be ready to advertise in the spring using ARRA funding.

The replacement is steel beam construction with a composite deck. The joints in the deck are now over the backwalls at the abutments and the deck is continuous over the pier. The bearings will be replaced with elastomeric bearings. The design was completed using the AASHTO LRFD Bridge Design Specifications and RIDOT LRFD Bridge Design Manual.



**Project Cost:** \$2.1 million

25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT			
<b>a.</b>	(1) FIRM NAME <b>Maguire Group Inc.</b>	(2) FIRM LOCATION <i>(City and State)</i> <b>Providence, RI</b>	(3) ROLE <b>Prime</b>
<b>b.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>c.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>d.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT**

*(Present as many projects as requested by the agency, or 10 projects, if not specified. Complete one Section F for each project.)*

**20. EXAMPLE PROJECT KEY NUMBER**  
10

<b>21. TITLE AND LOCATION</b> <i>(City and State)</i> Joseph A. Russo Memorial Bridge Rehabilitation	<b>22. YEAR COMPLETED</b>	
	<b>PROFESSIONAL SERVICES</b> 1998	<b>CONSTRUCTION</b> <i>(if applicable)</i> 2000

**23. PROJECT OWNER'S INFORMATION**

<b>a. PROJECT OWNER</b> Rhode Island Department of Transportation	<b>b. POINT OF CONTACT NAME</b> Robert Fura, PE	<b>c. POINT OF CONTACT TELEPHONE NUMBER</b> 401.222.2053 ext. 4107
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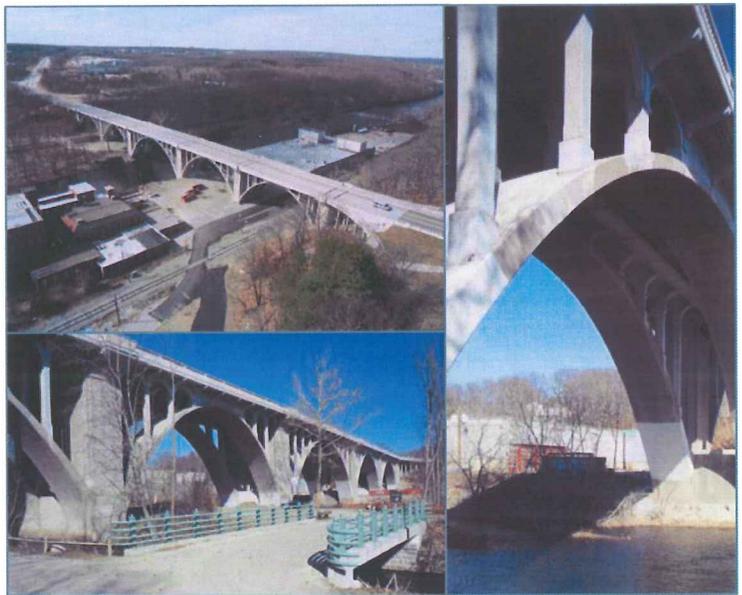
**24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT** *(Include scope, size, and cost.)*

Maguire Group Inc. performed the design for the reconstruction of the Joseph A. Russo Memorial Bridge (formerly the Ashton Viaduct) The bridge is a multi-span, reinforced concrete structure consisting of five major open spandrel arch spans with shorter approach spans at each end. The viaduct spans the Blackstone River and Canal as well as the main line of the Providence and Worcester Railroad. As a part of Rhode Island Route 116, it connects the Towns of Cumberland and Lincoln. The bridge (constructed in the 1930's and 1940's) has been declared eligible for the National Register of Historic Places.

The overall length of the viaduct is approximately 900' with the deck surface about 80' above the Blackstone River. The roadway width is 44' and carries four lanes of traffic - two in each direction.

All portions of the existing cast-in-place concrete structure, with the exception of the arch rings and piers, were replaced with precast concrete components using special field connections for accelerated bridge construction. Projecting rebars of one precast component were inserted into holes in the adjoining component and grout ports were used to connect the two. Also, precast concrete deck form-panels were used for the same accelerated construction purpose.

An extensive seismic evaluation of the structure was performed. Seismic isolation bearings and "friction-less" teflon surfaces will be used to mitigate the effects of seismic loading.



The Project received PCI's Award for "Excellence in Architectural and Engineering Design", ACI's Heavy Construction Award in recognition of creative use of concrete and Rhode Island Historical Preservation and Heritage Commissions Project Preservation Award.

**Project Cost:** \$12.5 million

**25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT**

<b>a.</b>	(1) FIRM NAME <b>Maguire Group Inc.</b>	(2) FIRM LOCATION <i>(City and State)</i> <b>Providence, RI</b>	(3) ROLE Prime
<b>b.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>c.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE
<b>d.</b>	(1) FIRM NAME	(2) FIRM LOCATION <i>(City and State)</i>	(3) ROLE

**G. KEY PERSONNEL PARTICIPATION IN EXAMPLE PROJECTS**

26. NAMES OF KEY PERSONNEL (From Section E, Block 12)	27. ROLE IN THIS CONTRACT (From Section E, Block 13)	28. EXAMPLE PROJECTS LISTED IN SECTION F (Fill in "Example Projects Key" section below, before completing table. Place "X" under project key number for project participation same or similar role.)									
		1	2	3	4	5	6	7	8	9	10
David Freeman, PE	Quality Control / Quality Assurance	X	X	X	X	X	X	X	X	X	X
Yihui Peter Wu, PhD, PE	Project Manager	X	X	X	X	X	X	X	X	X	X
Marty Pierce, PE	Lead Bridge / Structural Engineer	X		X	X	X	X	X	X	X	
Matthew Weidele, PE	Bridge / Structural Engineer	X	X	X	X	X	X	X	X	X	
Skyler Mills, PE	Lead Highway Design	X			X	X	X				
William Young, PE	Environmental Manager	X		X							
Jennifer James, PS	Environmental Scientist										
Jeffrey Lemay	Hydrology Engineer										

**29. EXAMPLE PROJECTS KEY**

No.	TITLE OF EXAMPLE PROJECT (FROM SECTION F)	No.	TITLE OF EXAMPLE PROJECT (FROM SECTION F)
1.	I-195 Relocation The Iway Project Providence, RI	6.	Improvements to Interstate Route 195 Contract 9 – Relocated I-195, India Point Park Pedestrian Bridge and Bike Path Underpass
2.	Route 2 over 2A Superstructure Bridge Replacement, Phillipston, MA	7.	I-195 Interstate Relocation Contract 11 Bridges over I-95 Design and Construction Management
3.	Cedar Street over 2A Route 9 Bridge Replacement D/B, Wellesley, MA	8.	I-95 Service Roads Garden, Pine, and George Street Pawtucket, RI
4.	Improvements to Interstate Route 195 Contract 6 – Ramps NE, WS & WES	9.	Repair/Replacement Wilbur Road Bridge
5.	Improvements to Interstate Route 195 Contract 8–Ramps SE, WN, WP & NP	10.	Joseph A. Russo Memorial Bridge Rehabilitation

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## H. ADDITIONAL INFORMATION

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30. PROVIDE ANY ADDITIONAL INFORMATION REQUESTED BY THE AGENCY. ATTACH ADDITIONAL SHEETS AS NEEDED.

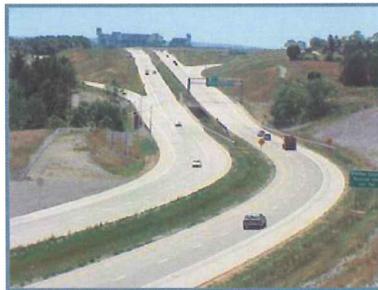
### TRANSPORTATION



Maguire's **Transportation Division** has made an indelible imprint on the transportation infrastructure of the Northeast and Mid-Atlantic. From the design of portions of Boston's massive Central Artery Project or Providence's "Iway" reconfiguration of two major interstates (I-195 and I-95), to construction management of Pennsylvania's I-99 and literally hundreds of smaller roadway and bridge projects, Maguire Group has played a significant role in the development of the nation's transportation network so essential to American prosperity and our way of life.



Maguire is proud that we have cultivated a reputation as the firm our clients think of first. That reputation is a clear manifestation of the quality of our work, the experience and expertise of our staff, the creativity of our transportation solutions, and our responsiveness to clients' needs.



With the vast in-house resources at our disposal, Maguire has the capability of performing all or any portion of the infrastructure design process from initial feasibility studies and design study reports right through permitting and on to the final project plans, specifications and estimates. This capability includes demonstrated abilities in coordinating among the many and varied agencies, departments, utility companies, town administrations and other interested parties that today's transportation projects require.

#### Transportation Services

- Bridge engineering
- Corridor studies for highway development
- Environmental assessments
- Geotechnical engineering
- Highway / roadway design and engineering
- Hydraulics and drainage design
- Pavement engineering
- Public participation
- Streetscapes
- Traffic engineering
- Traffic improvement and site access studies
- Transportation planning

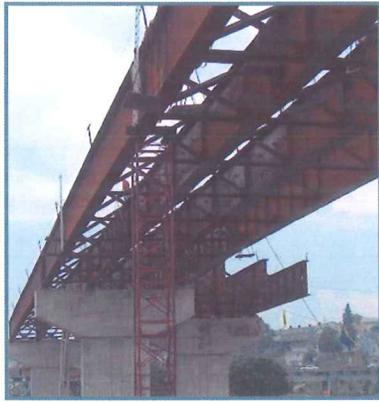


### BRIDGE ENGINEERING

Maguire Group's experienced **Bridge Design Unit** has tackled some of the more complex projects in the Northeast and Mid-Atlantic. These engineers have designed numerous bridges of various materials, span lengths and configurations, from single-span rural crossings to complex multi-span expressway and urban arterial crossings. Maguire's experience in the design of new bridges, replacement bridges, and bridge rehabilitation provides customers with the unparalleled knowledge and experience to support their project, small or large. Further complementing our design services, our inspection and maintenance capabilities are provided throughout the Eastern Seaboard.

#### Bridge Design

- Accelerated bridge construction techniques
- Curved girders
- Drilled shaft foundations
- Integral abutments
- LRFD and LRFR experience



- Pile foundations
- Post-tensioned pier caps
- Prestressed concrete box beams and bulb tees
- Road / pedestrian / rail bridges
- Seismic design
- Simple and continuous superstructures
- Steel and concrete arch bridges
- Steel tub girders
- Steel plate girders

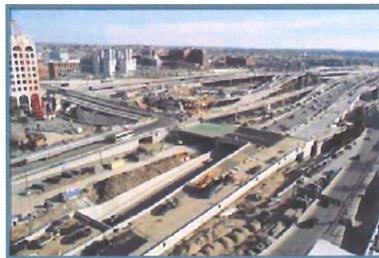
#### Bridge Rehabilitation & Construction

- Bridge inspection, testing and evaluation
- Construction inspection / management
- Eliminating joints
- Repairs and widenings
- Seismic retrofitting
- Structural analysis
- Structural evaluation and rating



#### Stream & River Crossings

- Highways and railroad bridges over major U.S. waterways
- Hydraulics



## HIGHWAY AND ROADWAY ENGINEERING

Our **transportation planning, design, and construction** efforts create and support the infrastructure that effectively moves people and goods.

Maguire's professionals design roadways that complement their environments, balancing multi-modal needs safely and efficiently. Our portfolio of thousands of highways, rural roads, urban streets, toll roads, and multi-modal systems encompasses every conceivable climate and terrain. Maguire's transportation specialists are enhanced by an array of environmental, geotechnical, and site drainage expertise.

Maguire's approach, developed through our involvement in hundreds of projects in every varied climates and terrains, provides clients with designs that meet current and projected needs in a cost-effective and sensitive manner.



#### Highway / Roadway Design and Engineering

- Asset management
- Conceptual, preliminary and detailed design
- Construction administration
- Drainage design
- Interchange planning and design
- Mechanically stabilized earth (MSE) wall design
- Pavement engineering and research
- Signing and pavement markings
- Street lighting
- Traffic signal systems
- Urban and rural roadways

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**I. AUTHORIZED REPRESENTATIVE**

The foregoing is a statement of facts.

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**31. SIGNATURE**

**32. DATE**

April 12, 2011

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**33. NAME AND TITLE**

**Richard Bernardo, PE – Senior Vice President**

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# ARCHITECT – ENGINEER QUALIFICATIONS

1. SOLICITATION NUMBER (If any)

## PART II – GENERAL QUALIFICATIONS

(If a firm has branch offices, complete for each specific branch office seeking work.)

2a. FIRM (OR BRANCH OFFICE) NAME <b>Maguire Group Inc.</b>			3. YEAR ESTABLISHED 1938	4. DUNS NUMBER 78-304-6287
2b. STREET 225 Chapman Street, 4 <sup>th</sup> Floor			5. OWNERSHIP	
2c. CITY Providence			2d. STATE RI	2e. ZIP CODE 02905
6a. POINT OF CONTACT NAME AND TITLE <b>Yihui Peter Wu, PE, PhD – Project Manager</b>			a. TYPE Corporation	
6b. TELEPHONE NUMBER 401/272-6000		6c. E-MAIL ADDRESS YWu@maguiregroup.com		
8a. FORMER FIRM NAME(S) (If any) NA			7. NAME OF FIRM (If block 2a is a branch office) Maguire Group Inc.	b. SMALL BUSINESS STATUS NA
			8b. YR ESTABLISHED NA	8c. DUNS NUMBER NA

9. EMPLOYEES BY DISCIPLINE				10. PROFILE OF FIRM'S EXPERIENCE AND ANNUAL AVERAGE REVENUE FOR LAST 5 YEARS		
a. Function Code	b. Discipline	c. No. of Employees		a. Profile Code	b. Experience	c. Revenue Index Number (see below)
		(1) FIRM	(2) BRANCH			
24	Environmental Scientist	6	3	B02	Bridges	5
12	Civil Engineer	11	2	C07	Coastal Engineering	2
60	Transportation	30	10	C14	Conservation and Resource Management	1
57	Structural	13	10	D08	Dredging Studies and Design	4
				E09	Environmental Impact Studies, Assessments or Statements	4
				E11	Environmental Planning	1
				E13	Environmental Testing and Analysis	2
				F02	Field Houses; Gyms; Stadiums	1
				H01	Harbors; Jetties; Piers, Ship Terminal Facilities	6
				H03	Hazardous, Toxic, Radioactive Waste Remediation	2
				H07	Highways; Streets; Airfield Paving; Parking Lots	8
				P06	Planning (Site, Installation, and Project)	6
				R06	Rehabilitation (Buildings; Structures; Facilities)	1
				S04	Sewage Collection, Treatment and Disposal	2
				S09	Structural Design; Special Structures	2
				T03	Traffic & Transportation Engineering	5
				U02	Urban Renewals; Community Development	1
				W03	Water Supply; Treatment and Distribution	1
	Other Employees	143	13			
<b>Total</b>		<b>185</b>	<b>38</b>			

11. ANNUAL AVERAGE PROFESSIONAL SERVICES REVENUES OF FIRM FOR LAST 3 YEARS (Insert revenue index number shown at right)		PROFESSIONAL SERVICES REVENUE INDEX NUMBER			
a. Federal Work	7	1. Less than \$100,000	6. \$2 million to less than \$5 million	7. \$5 million to less than \$10 million	8. \$10 million to less than \$25 million
b. Non-Federal Work	9	3. \$250,000 to less than \$500,000	8. \$10 million to less than \$25 million	9. \$25 million to less than \$50 million	10. \$50 million or greater
c. Total Work	9	4. \$500,000 to less than \$1 million	5. \$1 million to less than \$2 million		

### 12. AUTHORIZED REPRESENTATIVE

The foregoing is a statement of facts.

a. SIGNATURE 	b. DATE April 12, 2011
c. NAME AND TITLE <b>Richard Bernardo, PE – Senior Vice President</b>	



# CERTIFICATE OF LIABILITY INSURANCE

OP ID: M6

DATE (MM/DD/YYYY)

04/13/11

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

**IMPORTANT:** If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

<b>PRODUCER</b> <b>Regions Insurance, Inc.(Fay)</b> <b>1465 E.Joyce Blvd, Suite 205</b> <b>Fayetteville, AR 72703</b> <b>Liam R. Murphy</b>		<b>479-684-5250</b>  <b>479-684-5252</b>	<b>CONTACT NAME:</b> <b>PHONE (A/C, No, Ext):</b> <b>E-MAIL ADDRESS:</b> <b>PRODUCER CUSTOMER ID #: METRI-2</b>	<b>FAX (A/C, No):</b>
<b>INSURED</b> <b>Maguire Group, Inc.</b> <b>13940 SW 136th Street, Ste 103</b> <b>Miami, FL 33186</b>		<b>INSURER(S) AFFORDING COVERAGE</b>		<b>NAIC #</b>
		<b>INSURER A : Nat'l Fire Ins Co of Hartford</b>		<b>20478</b>
		<b>INSURER B : Charter Oak Fire Ins Co</b>		<b>25615</b>
		<b>INSURER C : Transportation Insurance Co</b>		<b>20494</b>
		<b>INSURER D : Travelers Prop Cas Co of Amer</b>		<b>25674</b>
		<b>INSURER E : Lexington Insurance Co</b>		<b>19437</b>
		<b>INSURER F :</b>		

**COVERAGES****CERTIFICATE NUMBER:****REVISION NUMBER:**

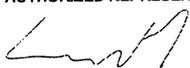
THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSR	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS	
A	GENERAL LIABILITY	X	X	4027144841	03/01/11	03/01/12	EACH OCCURRENCE	\$ 1,000,000
	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY						DAMAGE TO RENTED PREMISES (Ea occurrence)	\$ 50,000
	<input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR						MED EXP (Any one person)	\$ 5,000
							PERSONAL & ADV INJURY	\$ 1,000,000
							GENERAL AGGREGATE	\$ 2,000,000
							PRODUCTS - COMP/OP AGG	\$ 2,000,000
								\$
B	AUTOMOBILE LIABILITY	X	X	P8105480X631COF11	03/01/11	03/01/12	COMBINED SINGLE LIMIT (Ea accident)	\$ 1,000,000
	<input checked="" type="checkbox"/> ANY AUTO						BODILY INJURY (Per person)	\$
	<input type="checkbox"/> ALL OWNED AUTOS						BODILY INJURY (Per accident)	\$
	<input type="checkbox"/> SCHEDULED AUTOS						PROPERTY DAMAGE (Per accident)	\$
	<input checked="" type="checkbox"/> HIRED AUTOS							\$
	<input checked="" type="checkbox"/> NON-OWNED AUTOS							\$
C	<input checked="" type="checkbox"/> UMBRELLA LIAB <input checked="" type="checkbox"/> OCCUR	X		C4029279461	03/01/11	03/01/12	EACH OCCURRENCE	\$ 5,000,000
	<input type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE						AGGREGATE	\$ 5,000,000
	<input type="checkbox"/> DEDUCTIBLE							\$
	<input checked="" type="checkbox"/> RETENTION \$							\$
D	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY	Y/N	N/A	PJUB5525X88611	03/01/11	03/01/12	<input checked="" type="checkbox"/> WC STATUTORY LIMITS <input type="checkbox"/> OTHER	
	ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH)						E.L. EACH ACCIDENT	\$ 1,000,000
	If yes, describe under DESCRIPTION OF OPERATIONS below						E.L. DISEASE - EA EMPLOYEE	\$ 1,000,000
							E.L. DISEASE - POLICY LIMIT	\$ 1,000,000
E	Professional Liab			03142-8218	03/01/11	03/01/12	Ea Claim	1,000,000
	Contractor's Poll						CLAIMS-MADE FORM	Pol Agg

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (Attach ACORD 101, Additional Remarks Schedule, if more space is required)

Laurel Ave Bridge Replacement D/B Project  
A.A. Will Corporation is Additional Insured with respect to General Liability, Auto Liability and Umbrella Liability, with Waiver of Subrogation in favor of the Contractor, only as their interest may appear in signed

**CERTIFICATE HOLDER****CANCELLATION**

<b>AAWIST1</b>  <b>A. A. Will Corporation</b> <b>145 Island Street</b> <b>Stoughton, MA 02072</b>	SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.  AUTHORIZED REPRESENTATIVE 
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## **Resume - Lamson Engineering Corporation**

Name: Mr. Kin C. Lam, P.E.

Title: Principal

Education: M.S., U. of Wisconsin-Madison, Civil (Structural)  
M.S., U. of Michigan, Ann Arbor, Civil (Geotechnical)  
B.S., National Taiwan University, Civil

Professional Registration: P.E. in Massachusetts & Rhode Island

Experience:

Mr. Lam has over 25 years of experience in a wide range of geotechnical, structural and civil engineering projects. He has managed and designed many projects for Massachusetts Department of Transportation (MassDOT) from the preliminary design through final design to construction. Preparation of reports including those of geotechnical, construction plans, specifications and estimates are part of the design effort. The recent projects he was responsible for included the following:

- RIAC Reconstruction Runway 16-34, Newport Airport, Contract #22453, responsible for the boring program, testing and geotechnical report preparation for this 2009 airport runway reconstruction project (via MaFarland Johnson).
- Amtrak Wellington Signal Bridge of Warwick Intermodal Train Station, Cranston, RI, responsible for the boring program, foundation analysis & geotechnical report (via Jacobs).
- MassDOT Design-Build Cedar Street over Route 9, Wellesley Project responsible for boring program development, foundation analysis and geotechnical report preparation for this 2-span bridge replacement project (via Gill Engineering Associates).
- MassDOT Design-Build CSX Bridge Bundle Project, responsible for shallow foundation analysis and geotechnical report preparation for Rt. 148 Brookfield Bridge #B-26-003, Rt. 31 Charlton Bridge #C-06-014, Rt. 49 Spencer Bridge #S-23-024, and Rt. 67 West Brookfield Bridge #W-19-015 (via Fay, Spofford, & Thorndike).
- MassDOT Design-Build Lowell Bridge Bundle Project, I-495 northbound and southbound Bridges over Concord River, B&M & Woburn Street, Lowell, responsible for the integral and MSE abutments utilizing steel H-Piles and geogrid materials for these 6 bridges. This is the first MDOT project using such substructure type. (via AECOM).
- MassDOT Route 128 Add-A-Lane Project, Bridges IV for 7 bridges, responsible in boring program development, foundation analysis and geotechnical report preparation including the recommendation of the foundation types for this complex project. Proper shallow foundation, steel pile, & drilled shaft types were used for 7 bridges at Rte 109, Rte 135, Charles River & Great Plain Ave. (via Louis Berger Group).
- Route 2A Reconstruction Project, Town of Arlington responsible in roadway geotechnical foundation design and recommendations due to previous landfill underneath the existing roadway (via Fay, Spofford, & Thorndike).
- MassDOT Replacement Route 10 Bridge # E-05-006 over Manhan River, Easthamton, Mass., responsible for the geotechnical design and report preparation including the recommendation of the drilled micro-pile foundation for this single span bridge replacement project (via Diversified Technology Consultants).

## **Resume - Lamson Engineering Corporation**

**Name:** Mr. Suthichai Saelim

**Title:** Design Engineer

**Education:** MS, U. of Massachusetts - Lowell / 2001 / Civil  
BS, Khon Kaen University, Thailand, 1998 / Civil

### **Experience:**

Mr. Saelim has 10 years of experience in infrastructure projects with the company. His experience includes boring inspection, foundation analysis and design, construction plans and specifications preparation, quantities and cost estimates for Massachusetts Department of Transportation (MassDOT) bridge rehabilitation and replacement projects; analysis and report preparation for the load rating assignments; and inspection and report preparation for inspection projects of MassDOT & Massachusetts Bay Transportation Authority (MBTA). His geotechnical experience includes the following:

- RIAC Reconstruction Runway 16-34, Newport Airport, Contract #22453, responsible for the boring program, testing and geotechnical report preparation for this 2009 airport runway reconstruction project (via MaFarland Johnson).
- Amtrak Wellington Signal Bridge of Warwick Intermodal Train Station, Cranston, RI, responsible for the boring program, foundation analysis & geotechnical report (via Jacobs).
- MassDOT Design-Build Cedar Street over Route 9, Wellesley Project responsible for boring program development, foundation analysis and geotechnical report preparation for this 2-span bridge replacement project (via Gill Engineering Associates).
- MassDOT Design-Build CSX Bridge Bundle Project, responsible for shallow foundation analysis and geotechnical report preparation for Rt. 148 Brookfield Bridge #B-26-003, Rt. 31 Charlton Bridge #C-06-014, Rt. 49 Spencer Bridge #S-23-024, and Rt. 67 West Brookfield Bridge #W-19-015 (via Fay, Spofford, & Thorndike).
- MassDOT Design-Build Lowell Bridge Bundle Project, I-495 northbound and southbound Bridges over Concord River, B&M & Woburn Street, Lowell, responsible for the integral and MSE abutments utilizing steel H-Piles and geogrid materials for these 6 bridges. This is the first MDOT project using such substructure type. (via AECOM).
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## ANDREW J. PETERSON

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Bristol, RI

Office: 401.683.6630

Fax. 401.683.6630

E. [ajpeterson@nei-cds.com](mailto:ajpeterson@nei-cds.com)

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### Professional Experience:

Position: Surveyor, Party Chief

20+ years of field surveying experience, property line surveying, land evidence research, survey plan preparation and reconciliation along with survey crew staff oversight and training.

### Summary of Professional Experience

- **3/87 to Present at Narragansett Engineering, Inc.**
  - Employed as surveyor; duties include field surveys, preparation of subdivision and construction plans with utility designs using CAD system, training and supervision of field staff.  
Operations control of Total Station Transit, GPS Equipment, Data Collectors and Surveying Systems.  
Focus on surveying efficiency, one person survey systems.
  
- **Haley & Aldrich-CSO East and West - Providence - Geological Inspector**  
to monitor/test soils; classified soils and bedrock for a ten-month period.  
Willett Avenue Project - Party Chief for East Providence field survey crew to provide controls for aerial photogrammetry and performed cross-sections survey.  
Route 6 Upgrade - Party Chief for field survey crew to provide controls for aerial photogrammetry. Performed numerous surveys and prepared CAD plans for residential, commercial and industrial projects
  
- **10/83 to 3/87 Newport City Hall (Engineering Division)**  
Employed as draftsman. Duties included preparation of construction plans and profiles, details, and various city maps. Prepared and checked plans and specifications of work to be performed by the city as well as those designed by consulting engineers.

### Education

- 1996 Community College of Rhode Island (CCRI)  
Continued Course Work in Engineering
  
- 9/90 to 1993 Community College of Rhode Island

### Course Work in Engineering

- 10/85 to 12/86 Community College of Rhode Island  
Course Work in Engineering and Math
- 9/84 to 5/85 Hall Institute, 120 High St., Pawtucket, RI  
Graduated in the Mechanical Tool Design  
Drafting Program.
- 6/84 Graduated from Rogers High School, Newport RI

References available upon request.

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## **KAMAL HINGORANY**

President

### **Project Assignment**

Project Manager

### **Years of Experience**

30

### **Education**

B.S. Civil Engineering, University of Rhode Island, 1973.

### **Professional Affiliations**

Rhode Island Society of Professional Land Surveyors (RISPLS)

### **Professional Registrations**

Professional Engineer/Massachusetts

Professional Engineer/Rhode Island

Professional Land Surveyor/Rhode Island

### **Key Qualifications**

Kamal Hingorany has been the key person in establishing Narragansett Engineering, Inc. He is in charge of client interface and all engineering reviews. His field and office experience in Civil Engineering and Surveying has created an all-rounded company. His skills in other facets of management, such as accounting and knowledge of the complete CAD system, has brought Narragansett Engineering, Inc. to become the most successful and largest engineering company on Aquidneck Island in Rhode Island. He has successfully procured contracts from the Rhode Island Department of Transportation, Narragansett Bay Commission and local towns. Due to his efforts, the company is experiencing a rapid growth as larger projects are being procured. Kamal has assisted in all phases of metes and bounds survey projects and is involved with all cost estimates on surveying projects and their completion. Prior to immigrating to the United States, Kamal had gone through unit conversions from the British System (foot/pound) to the Metric System, while working in his father's consulting firm in India.

### **April, 1985 - Present**

Narragansett Engineering, Inc., (N.E.I.) Portsmouth, RI

President

Involved with all phases of residential, commercial and industrial projects. Supervisor of all projects and interface with clients. Projects consist of Civil and Structural design and detailing. Experienced in Civil, Structural and Environmental Engineering. Has testified and represented clients at various Town Planning and Zoning Board meetings pertinent to subdivisions, regulatory variances and special exceptions. Demands include designing and supervising Town sanitary sewer systems. NEI is currently involved with projects for RIDOT.

**Page 2 (Resume)**  
**KAMAL HINGORANY**  
**President**

**April, 1982 - April, 1985**

**United Engineers & Constructors, Inc. Seabrook, New Hampshire**

**Senior Engineer-** Supervision of design and installation of nuclear piping, pipe supports and electrical cable trays. As this project was a combination of design and field supervision, much of the time was spent in reviewing structures and connections in the field. Assignment also included review of documents for code conformance. The following code applications were practiced in this job assignment: AISC, ASME, Nuclear Piping Class I,II, III & B31.1.

**April, 1981 - April, 1982**

**Stone & Webster Engineering Corp., Boston, Massachusetts**

Worked as a Senior Analyst on nuclear piping. Supervised and analyzed Class I Piping for Shoreham Nuclear Power Plant. Class I analysis also requires evaluation of pipe welding and any attachment made to piping (lugs for pipe supports). Welding connections were analyzed as per AWS standards.

**January, 1980 - March, 1981**

**Engineering Analysis Services, East Greenwich, Rhode Island**

Worked as a Senior Analyst on nuclear piping and pipe supports. Assignment included analysis of Nuclear Piping Class II & III and designing pipe supports. I was also involved with field inspections and reviewing welding and bolt connections on site. This assignment enabled me to understand the procedures of weld certification and different methods used to evaluate defective welds.

**December, 1977 - January, 1980**

**ITT Grinnell Corp., Providence, Rhode Island**

Began as a Analyst and was promoted to Senior Analyst to design pipe supports. Work included analysis of Nuclear Piping Class II & III. Job description also included periodical site inspections of piping and pipe supports. Largest project on which I worked was for Bechtel at San Onofre Nuclear Power Plant. At times, long durations were spent at the site to review and resolve field problems. Welding seminars sponsored by Lincoln Electric were attended.

**Page 3 (Resume)**  
**KAMAL HINGORANY**  
**President**

**August, 1974 - November, 1977**

**Scientific Design Co., New York, New York.**

Began as a Structural Engineer. Worked on design of Petrochemical projects. My assignments consisted of designing various structural and reinforced concrete structures. These structural designs included designs of welded and bolted connections. These assignments provided me with a thorough knowledge of AISC, ACI, ASME and AWS codes and standards.

**January, 1973 - August, 1974**

**Romano Corp., East Providence, Rhode Island**

Project Engineer of industrial and commercial site development projects.

**Surveying Experience:**

**. Gray Craig Estates -**

1988 to 1993 - This project has been a most complex surveying project. Portions of subdivided land need to be dedicated and the easement rights are to be granted to the Town of Middletown and to the State of Rhode Island Department of Environmental Management. All aspects of the project were managed and survey documents reviewed for submission to appropriate agencies for land valuation.

**. Bittersweet Farm Subdivision -**

1986 to 1988 - Performed deed research; ran traverse and produce a metes and bounds survey; prepared subdivision plans for Planning Board approval; produced utility easements documents and dedication to the Town of Portsmouth, RI. Subsequent to this phase, he performed road and easements layout for construction and final dedication.

**. Sisson Pond Subdivision -**

1987 to 1988 - Performed deed research; prepared subdivision plans for Planning Board approval by the Town of Portsmouth.

In addition to the above mentioned projects, he has also reviewed many other large and small surveys, and deed descriptions.

**Additional Experience**

Kamal has worked as a Civil Engineer in petrochemical and nuclear industries under stringent Quality Control procedures. In fact, his job description included to help write, in part, the quality control procedures of a nuclear project. His past experience with structural analysis and nuclear plant design has been valuable in the growth of this company.

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# NEAL K. HINGORANY

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Portsmouth, RI 02871

Office. 401.683.6630

Fax. 401.683.6638

[Nhingorany@nei-cds.com](mailto:Nhingorany@nei-cds.com)

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## **Professional Experience:**

Narragansett Engineering Inc. Portsmouth, RI

2000-Present

Positions: Project Manager, Vice President

- Responsible for project management from proposals and estimation to project management and construction in various fields.

### Project scope includes:

- Property and Topographical Surveys  
Subdivision and Major Land Developments  
Environmental Analysis, Permitting, Impact Avoidance and Design  
Wastewater and Utility Systems Design  
Flood Hazard, Coastal Resource Analysis and Design
- Residential House Design, Commercial Design, Condominium Conversions  
Building Construction and Site Construction Supervision
- Daily operations of local (US based) and remote office with  
staff of Engineers, Architects, CAD draftspersons and administrative staff.
- Project accounts, accounts receivables, project cost estimating, construction cost estimating.

## **Qualifications:**

- Proficiency in AutoCAD (Focus on LDT, Civil / Survey, Architectural)
- Microsoft Office Suite (Excel, Access Database Management, etc.)
- Windows SB Server, office network management.
- Member Omicron Delta Upsilon
- Member RISPLS
- Building Construction Supervisor, Massachusetts.
- RS, Massachusetts [Registration no. 1362]
- LSIT, Rhode Island [Registration no. 2010-01]

## **Educational Experience:**

- University of Massachusetts at Dartmouth  
B.A. Economics (cum laude) 2004-2007
- University of Massachusetts at Amherst.  
College of Civil Engineering 1998-2001
- University of Rhode Island  
Selected post-graduate coursework (Environmental Science). 2010-ongoing

Note: References available by request.

## **Kathleen A. Gorman**

President

### **Experience Summary**

Over 23 years experience in construction management on projects ranging from \$0.5 million to \$500 million. Significant experience in CPM schedule development, review of contractor schedule submittals, analysis of contractor delay claims, schedule management and identification of mitigation measures to ensure timely completion of construction projects. Extensive use of Primavera Project Planner software and related comparison tools.

### **Schedule Dynamics, Inc.: May 2000 - Present**

#### **Checkpoint Consolidation and HVAC Improvements Terminal C – Logan Airport**

Client: Caminiti Consulting

Working on development of design schedule and conceptual construction schedule for renovations and additions to Terminal C at Logan Airport.

#### **Bridge Replacement Project-Bridge over Indian Head River Hanover-Hanson**

Client: A.A.Will Corporation

Development of baseline CPM schedule and monthly updates for Mass DOT bridge replacement project.

#### **Massachusetts Convention Center Authority-Misc. Renovations to Hynes and BCEC**

Client: Tishman Construction Corporation

This work includes analysis of contractor's baseline schedules and schedule updates for various renovations, both interior and exterior at the Hynes Convention Center, Boston Convention Center and Mass Mutual Center.

#### **McCourt Construction Company**

Schedule development, monitoring and time impact analysis for various McCourt projects.

#### **CRA International**

Served as schedule analyst on a consulting basis to CRA International. Work included delay analysis on a variety of construction claims and training of analysts in schedule review and analysis.

#### **Central Artery/Tunnel Project, Boston MA**

Client: Donovan Hatem LLP

Performed analysis of schedule related claims in connection with the overall management of the Central Artery/Tunnel project. Work included analysis of the integrated master project schedule, contractor's schedule submittals, pertinent schedule correspondence and development of a response to alleged schedule claims.

#### **Clay Center for the Performing Arts & Sciences, Charleston WV**

Client: CRA International

Provided CRA International a detailed window analysis of the Clay Center project schedule in defense of a claim filed against the project's architectural firm.

### Longwood Towers Project, Brookline MA

Client: Tishman Construction Corporation

Developed baseline schedule for renovation of condominium building, which included over 250 units, upgrades to the exterior façade and common areas.

### Boston Convention & Exhibition Center Project, Boston MA

Client: Tishman Construction Corporation

Duties included analysis of the contractor's baseline schedule; preparation of monthly reports, detailing construction progress; maintenance of the master development schedule for the project; evaluation of potential impacts to the schedule due to changes, design development or progress of the work. Reviewed and analyzed claims for productivity impacts and requests for extensions in contract time.

### Quorum Office Park, Chelmsford, MA

Client: Baraldi Construction, Inc.

Developed a baseline schedule for two new office buildings at Quorum Office Park, and updated these schedules monthly. Prepared schedule graphics and narrative of construction progress and critical items requiring attention for monthly reports.

### Other Projects

Client: Tishman Construction Corporation

Provided scheduling consulting services for the following projects:

- Historic Renovations to the John Adams Courthouse
- Logan Airport Terminal B – Security Checkpoint
- Rensselaer Polytechnic Institute – Experimental Media & Performing Arts Center
- Worcester Trial Court

### **PMA Consultants LLC: October 1990 - May 2000**

#### Central Artery/Tunnel Project, Boston, MA

Client: Bechtel/Parsons Brinckerhoff

- Developed the Technical Scope Statement for the Central Artery/Tunnel Project. This involved publication of a document identifying each construction contract, its location, contract limits, scope of work and interfacing contracts.
- Performed schedule development for the early utility relocation contracts and Interstate 93 underground tunnel contracts. This involved interpretation of conceptual design drawings and development of construction sequences, construction staging and work durations to maintain traffic and complete the construction projects within allotted time frames.
- Performed on-going analysis of the contractor's schedule for the \$250 million Bird Island Flats tunnel contract. Work included detailed analysis of the contractor's baseline schedule, monitoring of such schedule, analysis of claims and change orders for time entitlement for claims ranging from \$.2 - \$30 million and development of means to re-sequence work to meet the contract milestones.
- Developed and monitored the integrated project schedule required to meet an early opening of the Ted Williams Tunnel. This included identification of all contracts and work items requiring acceleration or work-arounds, monitoring progress and making plan adjustments through the successful opening of the tunnel in December 1995.
- Performed schedule analysis on the benefits of the early Ted Williams Tunnel opening, which successfully increased FHWA funding by \$15 million.

- On the \$165 million I-93 Tunnel Finishes contract, performed schedule development including definition of scope, sequencing and coordination of phased turnover of work areas from multiple construction contracts. This project included over 90 different access locations and 90 contract milestones. Performed analysis of the contractor's schedule submittals and monitoring of schedule updates.
- As a Schedule Analyst in the Claims and Changes group, performed detailed delay analysis of contractor claims on various construction contracts.

Logan 2000 – West Parking Garage and Elevated Walkways, Boston, MA

Client: Tishman Construction

- On the \$65 million West Parking Garage and the \$35 million Elevated Walkways contracts, performed comprehensive review of the contractor's baseline schedule and schedule updates, identified potential schedule impacts and possible means of mitigation, analyzed contractor delay claims and prepared Owner's position and response.

Pharmaceutical Laboratory, Florence, South Carolina

Client: Roche Carolina

- Performed itemized review of contractor claims on various contracts on this \$500 million project. This included detailed analysis of a number of projects, development of as-built schedules from daily construction reports, and evaluation of merit of such claims for this multi-contract project.

Other Projects:

- Additionally, served as a claims analyst for the Cambridge Housing Authority's Newtowne Court project, performed schedule reviews on the new \$60 million Boston City Hospital project and \$35 million Boston Police Headquarters project, performed schedule development for the Foxboro Company and performed claims review for the Town of Northbridge,

Turner Construction Company: July 1986 – September 1990

Norwalk, CT

- Served as Assistant Superintendent on the \$11 million St. Mary's Hospital Addition in Waterbury, CT. This assignment involved scheduling and field supervision of all trades for a two-story extension of an existing hospital, above the operating room floor. This required extensive coordination and communication with the hospital staff in order to maintain full use of the operating rooms while construction was in progress
- As a Project Management Control Systems Engineer, prepared and presented quarterly financial reports for projects valued at \$40 million. Organized monthly project meetings to discuss planning recommendations and forecast evaluations with senior management. Created CPM systems and various control systems necessary for resource management and forecasting manpower requirements.
- In the Special Projects Division, was solely responsible for all aspects of construction projects including estimating, purchasing, engineering, scheduling and all field coordination and supervision. Projects ranges from \$.25 - .8 million.
- Served as Field Engineer on the \$30 million Hyatt Regency Hotel in Greenwich, Connecticut.

Education

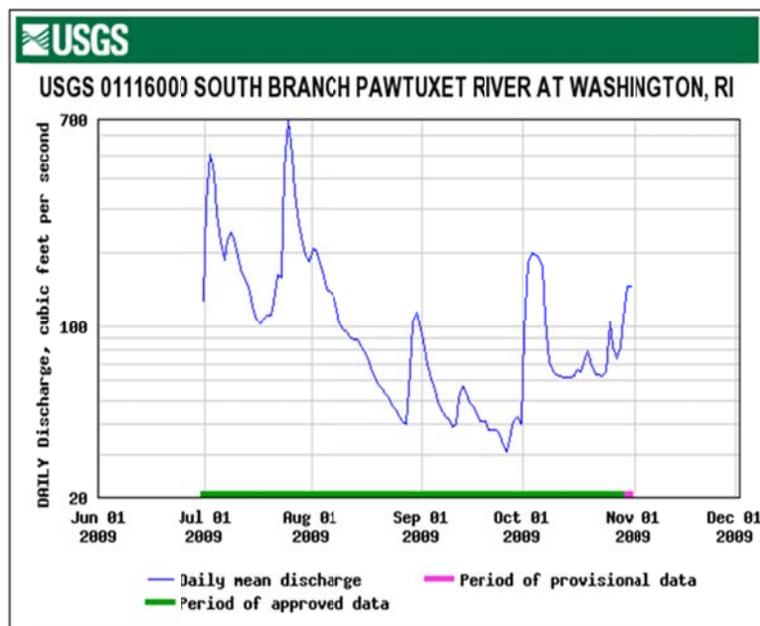
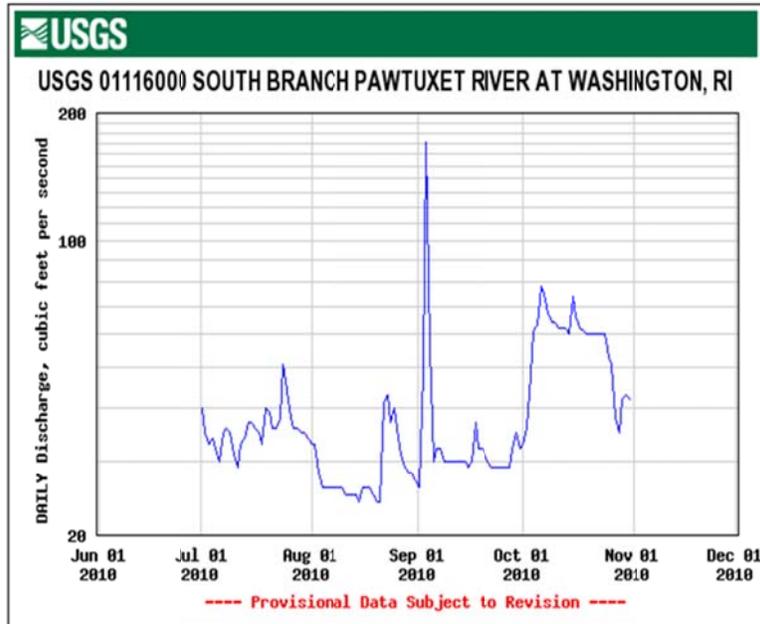
Bachelor of Science in Civil and Environmental Engineering, 1986  
Clarkson University, Potsdam, New York

## FLOW DATA



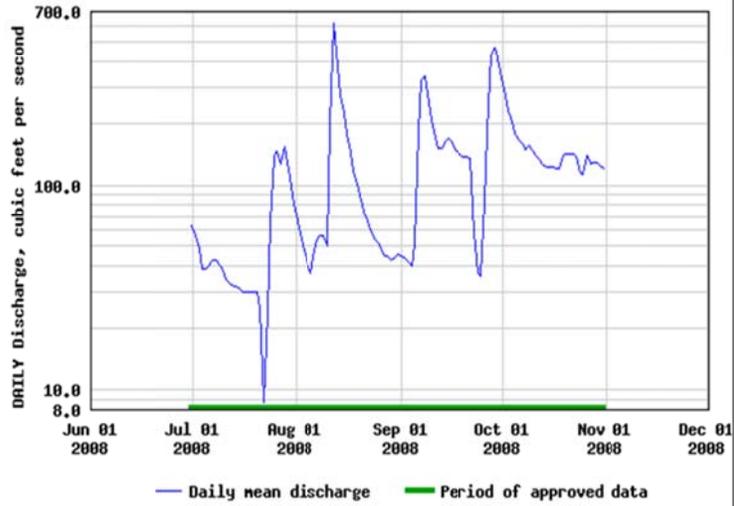
Map Depicting "USGS USGS 01116000 SOUTH BRANCH PAWTUXET RIVER AT WASHINGTON, RI" in vicinity to Laurel Avenue Bridge Site (approx.1.7 miles downstream from gage)

The following charts depict River flows at USGS 01116000 between the months of July and October for the past 10 years.

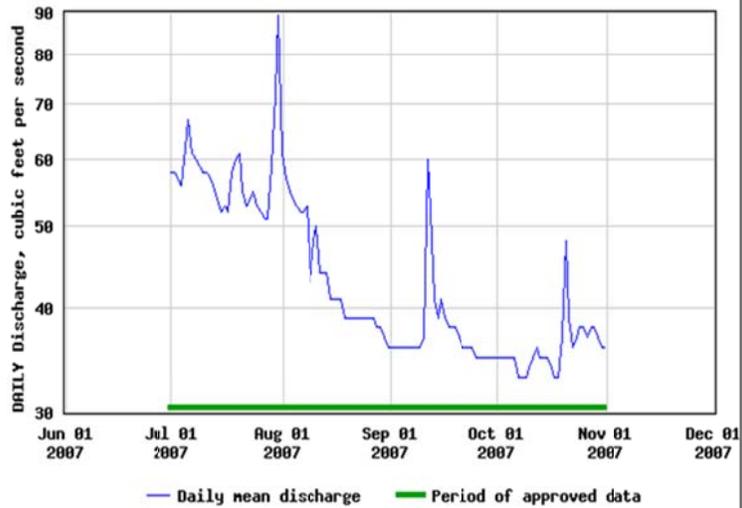


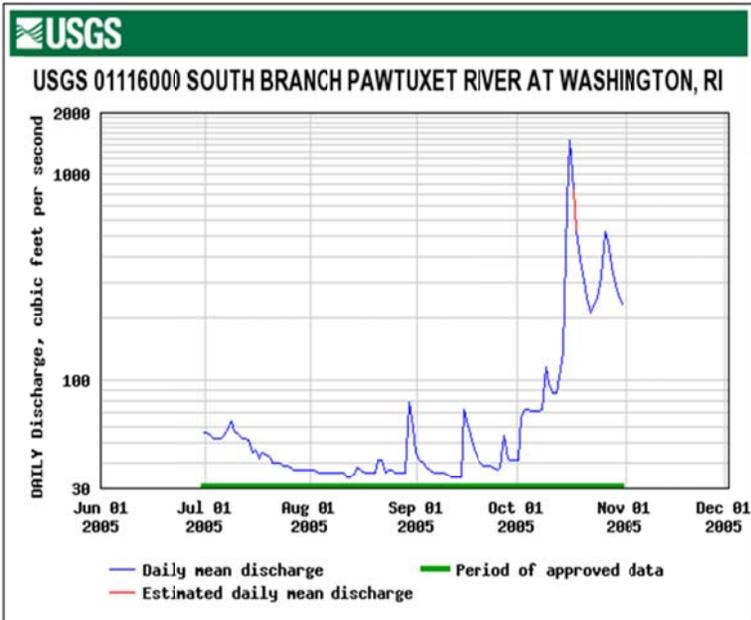
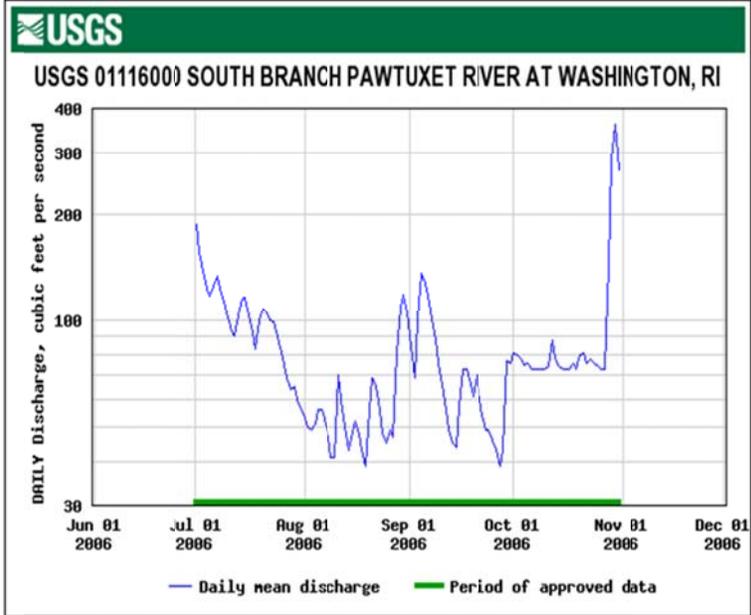


USGS 01116000 SOUTH BRANCH PAWTUXET RIVER AT WASHINGTON, RI



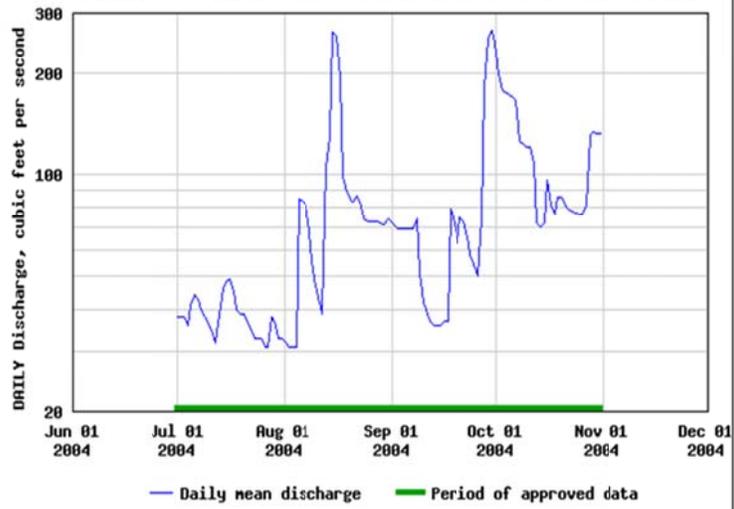
USGS 01116000 SOUTH BRANCH PAWTUXET RIVER AT WASHINGTON, RI



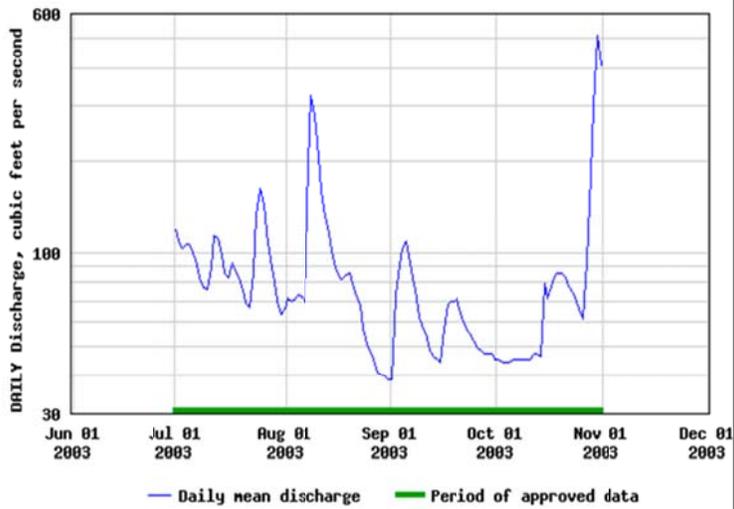




### USGS 01116000 SOUTH BRANCH PAWTUXET RIVER AT WASHINGTON, RI

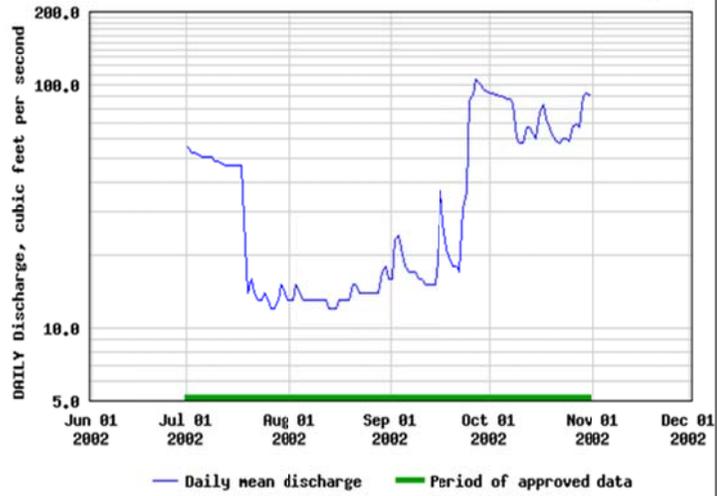


### USGS 01116000 SOUTH BRANCH PAWTUXET RIVER AT WASHINGTON, RI





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